

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION
JEF-FERNWOOD RD.
VILLAGE OF WINTERSVILLE
CROSS CREEK TOWNSHIP
JEFFERSON COUNTY

PROJECT DESCRIPTION

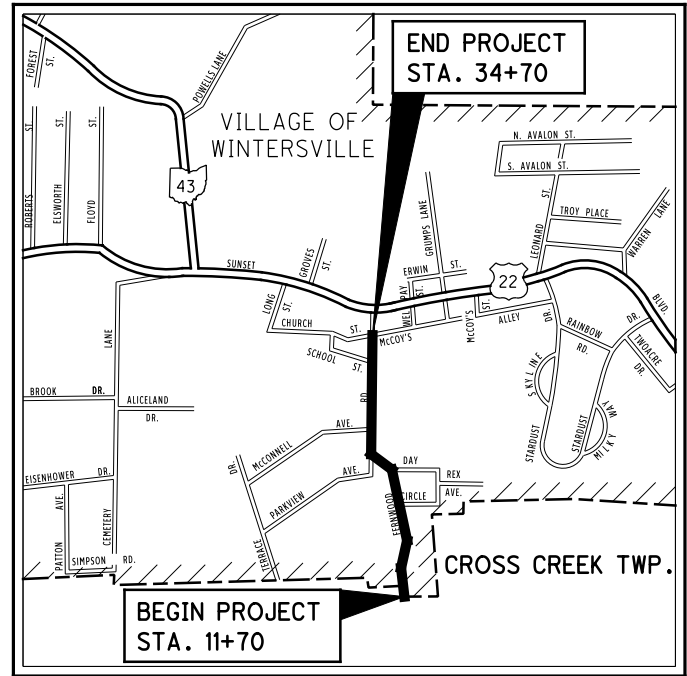
UPGRADING 0.44 MILE OF FERNWOOD ROAD BY WIDENING AND RESURFACING, INCLUDING NEW STORM SEWER SYSTEM, CURB AND GUTTER, SIDEWALK. TRAFFIC CONTROL SIGNS AND PAVEMENT MARKINGS.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: 3.1 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: 0.5 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA: 4.9 ACRES

2013 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.



LOCATION MAP

LATITUDE: 40°22'20" LONGITUDE: 80°42'05"



| | |
|----------------------------------|-------|
| PORTION TO BE IMPROVED | ----- |
| INTERSTATE & DIVIDED HIGHWAY | ===== |
| UNDIVIDED STATE & FEDERAL ROUTES | ==== |
| OTHER ROADS | ----- |

DESIGN DESIGNATION

| | | |
|-----------------------------------|-------|------------|
| CURRENT ADT (2013) | ----- | 2940 |
| DESIGN YEAR ADT (2025) | ----- | 4494 |
| DESIGN HOURLY VOLUME (2025) | ----- | 449 |
| DIRECTIONAL DISTRIBUTION | ----- | 50% |
| TRUCKS (24 HOUR B&C) | ----- | 3% |
| DESIGN SPEED | ----- | 3R PROJECT |
| LEGAL SPEED | ----- | 35 MPH |
| DESIGN FUNCTIONAL CLASSIFICATION: | | |
| 06 MINOR COLLECTOR (URBAN) | | |
| NHS PROJECT | ----- | NO |

DESIGN EXCEPTIONS

| DESIGN FEATURE | APPROVAL DATES | SHEET NUMBER |
|-------------------------|----------------|--------------|
| HORIZONTAL ALIGNMENT: | | |
| SUPERELEVATION | 6-27-14 | 2 |
| VERTICAL ALIGNMENT: | | |
| STOPPING SIGHT DISTANCE | 6-27-14 | 10, 12 |

INDEX OF SHEETS:

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| GENERAL NOTES | 5 |
| MAINTENANCE OF TRAFFIC | 6-7 |
| GENERAL SUMMARY | 8 |
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STANDARD CONSTRUCTION DRAWINGS

| STANDARD CONSTRUCTION DRAWINGS | | | | | | SUPPLEMENTAL SPECIFICATIONS | |
|--------------------------------|---------|---------|----------|-----------|----------|-----------------------------|---------|
| BP-1.1 | 7/28/00 | MGS-1.1 | 7/19/13 | MT-105.10 | 7/19/13 | 800-2015 | 1/21/15 |
| BP-2.1 | 7/19/13 | MGS-2.1 | 7/19/13 | | | 832 | 1/17/14 |
| BP-2.2 | 7/18/08 | MGS-4.2 | 7/19/13 | TC-41.20 | 10/18/13 | | |
| BP-3.1 | 7/18/14 | MGS-5.3 | 7/19/13 | TC-41.40 | 10/18/13 | | |
| BP-4.1 | 7/19/13 | | | TC-42.20 | 10/18/13 | | |
| BP-5.1 | 7/19/13 | HW-2.1 | 1/18/13 | TC-52.10 | 10/18/13 | | |
| BP-7.1 | 7/18/14 | HW-2.2 | 1/18/13 | TC-52.20 | 7/18/14 | | |
| | | | | TC-71.10 | 1/17/14 | | |
| CB-2.1 | 1/18/13 | LA-1.1 | 10/15/10 | | | | |
| CB-2.2 | 1/17/14 | LA-1.2 | 10/15/10 | | | | |
| CB-2.3 | 1/18/13 | | | | | | |
| | | MH-1.1 | 1/18/13 | | | | |
| DM-1.1 | 1/18/13 | MH-1.2 | 1/18/13 | | | REINFORCED EARTH | 5/27/14 |
| DM-4.4 | 7/20/12 | MH-1.3 | 1/18/13 | | | WATERWAY PERMIT | 1/23/15 |

UNDERGROUND UTILITIES
CONTACT BOTH SERVICES TWO WORKING DAYS BEFORE YOU DIG.

Call Before You Dig
1-800-362-2764

(Non-members must be called directly)
OIL & GAS PRODUCERS
UNDERGROUND PROTECTION SERVICE
1-800-925-0988

PLAN PREPARED BY:
JOHN J. DOE & ASSOC., INC.
CONSULTING ENGINEERS
9999 ENGLISH DRIVE
COMPUTERLAND, OHIO 00000

ENGINEERS SEAL:

SIGNED: *John J. Doe*
DATE: 11/11/14

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED _____
DATE _____ MAYOR, VILLAGE OF WINTERSVILLE

APPROVED _____
DATE _____ DISTRICT DEPUTY DIRECTOR

APPROVED _____
DATE _____ DIRECTOR, DEPARTMENT OF TRANSPORTATION

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

DAR-121-(21.73)(22.83)

**RICHLAND TOWNSHIP
DARKE COUNTY**

PROJECT DESCRIPTION

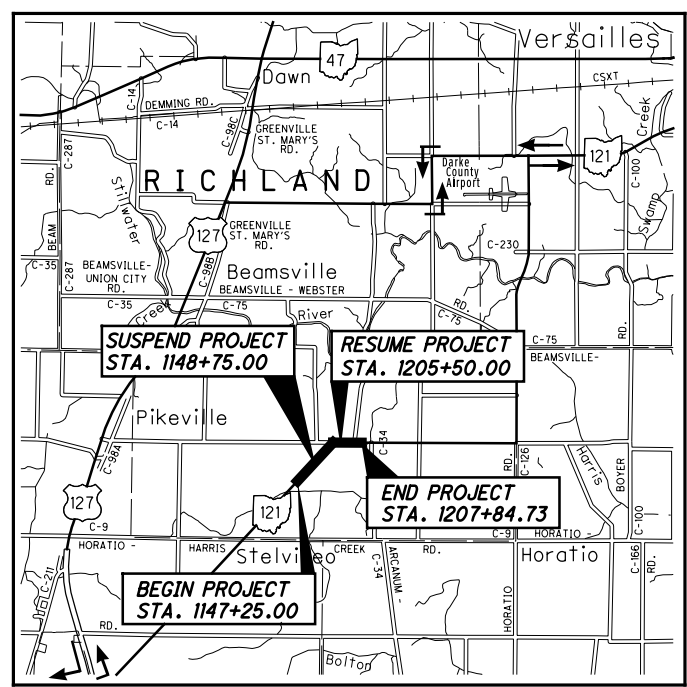
IMPROVEMENT OF 0.07 MILE OF STATE ROUTE 121 BY REPLACEMENT OF TWO STRUCTURES OVER A BRANCH OF THE STILLWATER RIVER, INCLUDING APPROACH RECONSTRUCTION.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: 5.4 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: 2.1 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA: 7.5 ACRES

2013 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.



LOCATION MAP

LATITUDE: 40°09'30" LONGITUDE: 84°34'05"



| | | |
|----------------------------------|-----|-------|
| PORTION TO BE IMPROVED | --- | ===== |
| INTERSTATE & DIVIDED HIGHWAY | --- | ===== |
| UNDIVIDED STATE & FEDERAL ROUTES | --- | ===== |
| OTHER ROADS | --- | ===== |
| DETOUR ROUTE | --- | ===== |

DESIGN DESIGNATION

| | | |
|-----------------------------------|-------|----------------------------|
| CURRENT ADT (2013) | ----- | 1460 |
| DESIGN YEAR ADT (2033) | ----- | 2040 |
| DESIGN HOURLY VOLUME (2033) | ----- | 204 |
| DIRECTIONAL DISTRIBUTION | ----- | 55% |
| TRUCKS (24 HOUR B&C) | ----- | 2% |
| DESIGN SPEED | ----- | 55 MPH |
| LEGAL SPEED | ----- | 55 MPH |
| DESIGN FUNCTIONAL CLASSIFICATION: | ----- | 05 MAJOR COLLECTOR (RURAL) |
| NHS PROJECT | ----- | NO |

DESIGN EXCEPTIONS

NONE REQUIRED

INDEX OF SHEETS:

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| GENERAL SUMMARY | 8 |
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| RIGHT OF WAY | 31-36 |
| SOIL PROFILES | |

ENGINEERS SEAL

FOR STRUCTURES OVER 20' SPAN



SIGNED: *John J. Doe*
DATE: 11-11-14

ENGINEERS SEAL

FOR ENTIRE PLAN EXCEPT STRUCTURES OVER 20' SPAN



SIGNED: *Jane C. Smith*
DATE: 11-11-14

STANDARD CONSTRUCTION DRAWINGS

| NO. | DATE | NO. | DATE | SUPPLEMENTAL SPECIFICATIONS |
|---------|----------|-----------|---------|-----------------------------|
| BP-3.1 | 7/18/14 | WO-1.1 | 1/18/13 | 800-2015 1/21/15 |
| BP-4.1 | 7/19/13 | WO-1.2 | 1/18/13 | 832 1/17/14 |
| DM-1.1 | 1/18/13 | AS-1-81 | 1/18/13 | |
| DM-4.1 | 7/19/13 | EXJ-4-87 | 7/19/02 | |
| | | PSBD-2-07 | 1/21/11 | |
| MGS-1.1 | 1/20/12 | TST-1-99 | 1/17/14 | |
| MGS-2.1 | 7/19/13 | | | |
| MGS-4.2 | 7/19/13 | | | |
| MGS-5.3 | 7/19/13 | | | |
| LA-1.1 | 10/15/10 | | | |
| LA-1.2 | 10/15/10 | | | |

SUPPLEMENTAL SPECIFICATIONS

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT DETOURS WILL BE PROVIDED AS INDICATED ON SHEET 1.

APPROVED _____
DATE _____ DISTRICT DEPUTY DIRECTOR

APPROVED _____
DATE _____ DIRECTOR, DEPARTMENT OF TRANSPORTATION

UNDERGROUND UTILITIES
CONTACT BOTH SERVICES TWO WORKING DAYS BEFORE YOU DIG.

Call Before You Dig
1-800-362-2764

(Non-members must be called directly)
OIL & GAS PRODUCERS UNDERGROUND PROTECTION SERVICE
1-800-925-0988

PLAN PREPARED BY:
JOHN J. DOE & ASSOC., INC.
CONSULTING ENGINEERS
9999 ENGLISH DRIVE
COMPUTERLAND, OHIO 00000

FEDERAL PROJECT NO. **NON-FEDERAL**

PID NO. **14653**

CONSTRUCTION PROJECT NO.

RAILROAD INVOLVEMENT **NONE**

DAR-121-(21.73)(22.83)

1/36

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

TUS-93-8.02

**RECONSTRUCTION OF EXISTING
SEPARATED CROSSING WITH THE
OHIO CENTRAL RAILROAD**

**WAYNE TOWNSHIP
TUSCARAWAS COUNTY**

PROJECT DESCRIPTION

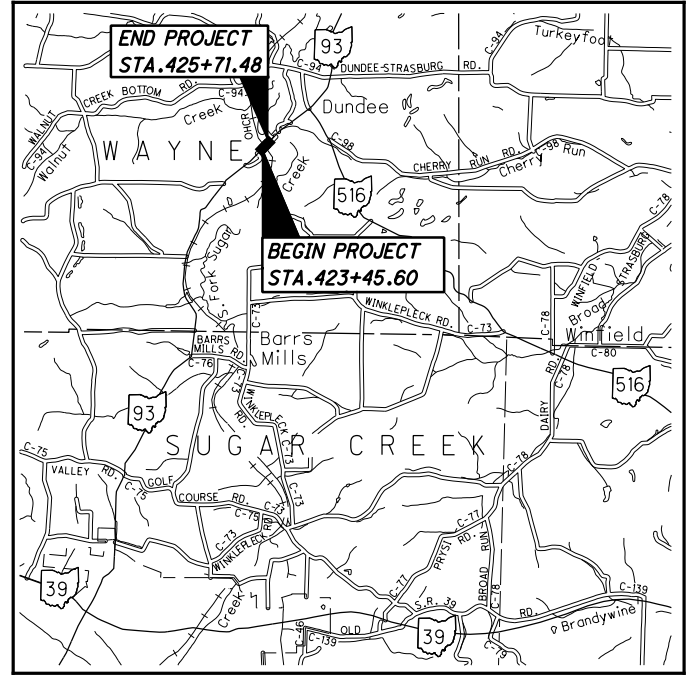
REHABILITATION OF THE EXISTING STRUCTURE
OVER THE OHIO CENTRAL RAILROAD BY REPLACE-
MENT OF THE BRIDGE DECK AND APPROACH SLABS.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: 1.5 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: 0.7 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA: 4.9 ACRES

2013 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF
OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING
SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS
AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN
THIS IMPROVEMENT.



LOCATION MAP

LATITUDE: 40°34'20" LONGITUDE: 81°37'00"



PORTION TO BE IMPROVED

| | |
|----------------------------------|-------|
| INTERSTATE & DIVIDED HIGHWAY | ===== |
| UNDIVIDED STATE & FEDERAL ROUTES | ===== |
| OTHER ROADS | ----- |

DESIGN DESIGNATION

| | | |
|-----------------------------------|-------|----------------------------|
| CURRENT ADT (2013) | ----- | 1270 |
| DESIGN YEAR ADT (2033) | ----- | 2240 |
| DESIGN HOURLY VOLUME (2033) | ----- | 224 |
| DIRECTIONAL DISTRIBUTION | ----- | 60% |
| TRUCKS (24 HOUR B&C) | ----- | 5% |
| DESIGN SPEED | ----- | 55 MPH |
| LEGAL SPEED | ----- | 55 MPH |
| DESIGN FUNCTIONAL CLASSIFICATION: | ----- | 06 MINOR COLLECTOR (RURAL) |
| NHS PROJECT | ----- | NO |

DESIGN EXCEPTIONS

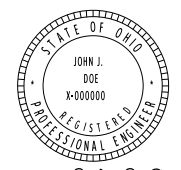
NONE REQUIRED

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ENGINEERS SEAL

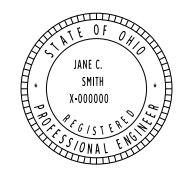
FOR STRUCTURES
OVER 20 FOOT SPAN



SIGNED: *John J. Doe*
DATE: 11/11/14

ENGINEERS SEAL

FOR ENTIRE PLAN
EXCEPT STRUCTURES
OVER 20 FOOT SPAN



SIGNED: *Jane C. Smith*
DATE: 11/11/14

STANDARD CONSTRUCTION DRAWINGS

| STANDARD CONSTRUCTION DRAWINGS | | | | SUPPLEMENTAL SPECIFICATIONS | |
|--------------------------------|---------|----------|---------|-----------------------------|---------|
| BP-3.1 | 7/18/14 | AS-1-81 | 1/18/13 | 800-2015 | 1/21/15 |
| BP-4.1 | 7/19/13 | CPA-1-08 | 7/18/08 | 832 | 1/17/14 |
| BP-5.1 | 7/19/13 | CS-1-08 | 7/18/08 | | |
| | | SBR-1-99 | 7/19/02 | | |
| DM-1.1 | 1/18/13 | | | | |
| DM-1.4 | 1/18/13 | MT-96.11 | 7/18/14 | | |
| DM-4.4 | 7/20/12 | MT-96.20 | 7/19/13 | | |
| | | MT-96.26 | 7/19/13 | | |
| MGS-1.1 | 1/20/12 | | | | |
| MGS-2.1 | 7/19/13 | | | | |
| MGS-3.1 | 7/18/14 | | | | |
| MGS-4.2 | 7/19/13 | | | | |
| MGS-5.3 | 7/19/13 | | | | |
| RM-4.2 | 7/19/13 | | | | |

SPECIAL PROVISIONS

WATERWAY PERMIT
1/23/15

I HEREBY APPROVE THESE PLANS AND DECLARE THAT
THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE
THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT
PROVISIONS FOR THE MAINTENANCE AND SAFETY OF
TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND
ESTIMATES.

APPROVED _____
DATE _____ DISTRICT DEPUTY DIRECTOR

APPROVED _____
DATE _____ DIRECTOR, DEPARTMENT OF
TRANSPORTATION

UNDERGROUND UTILITIES

CONTACT BOTH SERVICES TWO WORKING DAYS
BEFORE YOU DIG.

OHIO Utilities Protection SERVICE
Call Before You Dig
1-800-362-2764
(Non-members must be called directly)

OIL & GAS PRODUCERS UNDERGROUND PROTECTION SERVICE
1-800-925-0988

PLAN PREPARED BY:
JOHN J. DOE & ASSOC., INC.
CONSULTING ENGINEERS
9999 ENGLISH DRIVE
COMPUTERLAND, OHIO 00000

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

WYA-30-9.11
SALEM TOWNSHIP
WYANDOT COUNTY

PROJECT DESCRIPTION

CONSTRUCTION OF A 2.24 MILE CONNECTION FROM U.S. 30 TO U.S. 23, WITH A CONNECTING ROAD AT U.S. 30 AND A LIGHTED INTERCHANGE AT U.S. 23. INCLUDING CONSTRUCTION OF STRUCTURES U.S. 30 UNDER T.R. 49, U.S. 30 WB OVER U.S.23 SB, RAMP B, AND U.S. 23 UNDER C.R. 47; REPLACEMENT OF STRUCTURES U.S. 23 NB AND SB OVER LITTLE TYMOCHTEE CREEK; RECONSTRUCTION OF VARIOUS LOCAL ROADS; AND INSTALLATION OF NECESSARY TRAFFIC CONTROL DEVICES.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: 20.6 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: 5.4 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA: 26.0 ACRES

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

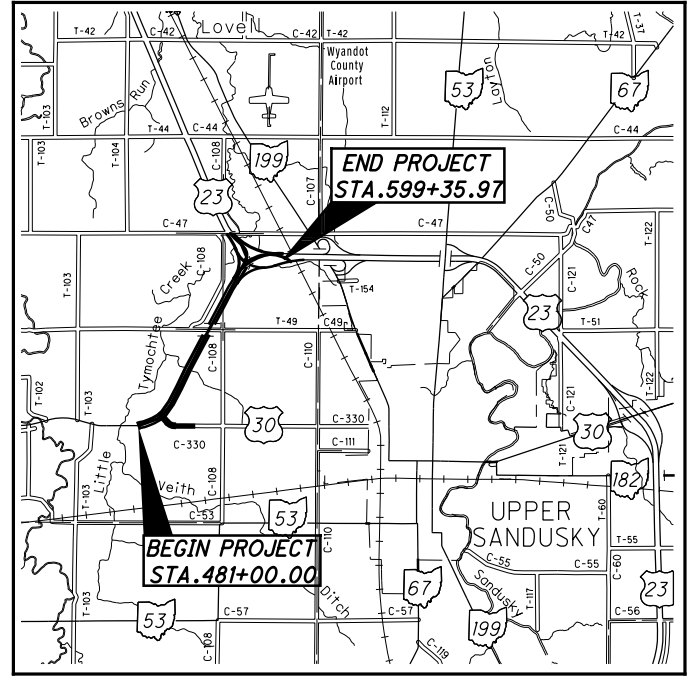
2013 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY EXCEPT AS NOTED ON SHEET 25, AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED _____
DATE _____ DISTRICT DEPUTY DIRECTOR

APPROVED _____
DATE _____ DIRECTOR, DEPARTMENT OF TRANSPORTATION



LOCATION MAP

LATITUDE: 40°50'50" LONGITUDE: 83°19'35"



PORTION TO BE IMPROVED

INTERSTATE & DIVIDED HIGHWAY

UNDIVIDED STATE & FEDERAL ROUTES

OTHER ROADS

DESIGN DESIGNATION

| | |
|-----------------------------------|--------------------|
| CURRENT ADT (2013) | 3510 |
| DESIGN YEAR ADT (2033) | 4880 |
| DESIGN HOURLY VOLUME (2033) | 488 |
| DIRECTIONAL DISTRIBUTION | 55% |
| TRUCKS (24 HOUR B&C) | 20% |
| DESIGN SPEED | 70 MPH |
| T _d | 20% |
| LEGAL SPEED | 55 MPH |
| DESIGN FUNCTIONAL CLASSIFICATION: | 06 FREEWAY (RURAL) |
| NHS PROJECT | NO |

DESIGN EXCEPTIONS

NONE REQUIRED

UNDERGROUND UTILITIES
CONTACT BOTH SERVICES TWO WORKING DAYS BEFORE YOU DIG.

OHIO Utilities Protection SERVICE
Call Before You Dig
1-800-362-2764
(Non-members must be called directly)

OIL & GAS PRODUCERS UNDERGROUND PROTECTION SERVICE
1-800-925-0988

PLAN PREPARED BY:
JOHN J. DOE & ASSOC., INC.
CONSULTING ENGINEERS
9999 ENGLISH DRIVE
COMPUTERLAND, OHIO 00000

| | |
|--|---|
| ENGINEERS SEAL FOR STRUCTURES OVER 20 FOOT | ENGINEERS SEAL FOR ENTIRE PLAN EXCEPT STRUCTURES OVER 20 FOOT |
| | |
| SIGNED: <i>John J. Doe</i> DATE: 11/11/14 | SIGNED: <i>Jane C. Smith</i> DATE: 11/11/14 |

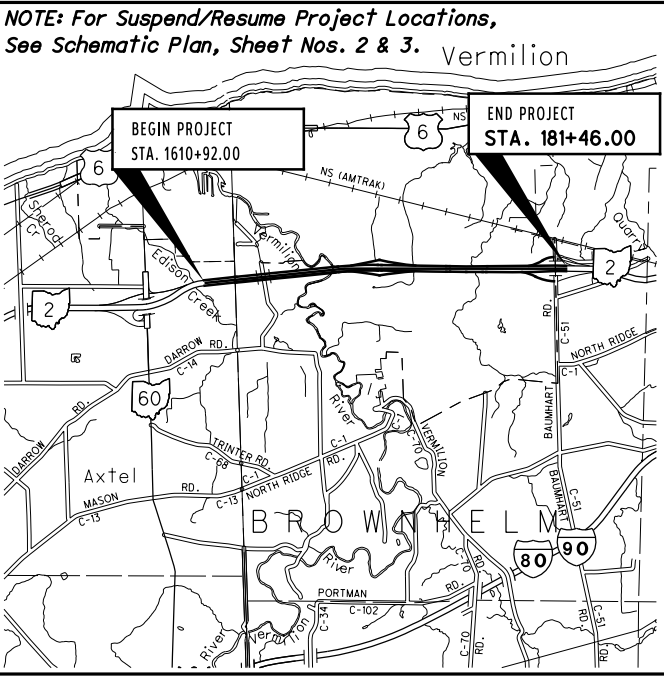
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| STANDARD CONSTRUCTION DRAWINGS | | | | | | | | SUPPLEMENTAL SPECIFICATIONS | | | |
|--------------------------------|---------|---------|----------|-----------|---------|----------|---------|-----------------------------|----------|----------|---------|
| BP-1.1 | 7/28/00 | MGS-1.1 | 7/19/13 | MH-1.1 | 1/18/13 | HL-10.31 | 1/16/15 | TC-07.65 | 10/18/13 | 800-2015 | 1/21/15 |
| | | MGS-2.1 | 7/19/13 | MH-1.2 | 1/18/13 | HL-20.11 | 1/16/15 | TC-12.30 | 10/18/13 | 832 | 1/17/14 |
| CB-2.2 | 1/17/14 | MGS-3.1 | 7/18/14 | MH-3.1 | 1/18/13 | HL-20.14 | 1/16/15 | TC-21.10 | 10/18/13 | | |
| CB-3.1 | 1/18/13 | MGS-4.2 | 7/19/13 | | | HL-20.21 | 1/17/14 | TC-21.20 | 1/16/15 | | |
| CB-3.2 | 1/18/13 | MGS-5.3 | 7/19/13 | RM-1.1 | 7/18/14 | HL-30.11 | 1/16/15 | TC-22.20 | 1/17/14 | | |
| CB-3.4 | 1/18/13 | MGS-6.1 | 7/19/13 | RM-4.2 | 6/4/14 | HL-30.21 | 1/17/14 | TC-41.10 | 7/19/13 | | |
| | | | | RM-4.3 | 7/18/14 | HL-30.22 | 1/17/14 | TC-41.20 | 10/18/13 | | |
| DM-1.1 | 1/18/13 | HW-1.1 | 1/18/13 | | | HL-30.31 | 1/17/14 | TC-41.40 | 10/18/13 | | |
| DM-4.4 | 7/20/12 | HW-2.1 | 1/18/13 | MT-95.30 | 7/18/14 | HL-40.10 | 1/17/14 | TC-41.50 | 10/18/13 | | |
| | | HW-2.2 | 1/18/13 | MT-102.10 | 7/18/14 | HL-50.11 | 1/16/15 | TC-42.10 | 10/18/13 | | |
| F-2.1 | 7/19/13 | | | | | HL-60.21 | 1/16/15 | TC-42.20 | 10/18/13 | | |
| F-3.1 | 7/19/13 | LA-1.1 | 10/15/10 | AS-1-81 | 1/18/13 | HL-60.31 | 1/16/15 | TC-51.11 | 1/17/14 | | |
| F-3.3 | 7/19/13 | LA-1.2 | 10/15/10 | ICD-1-82 | 7/19/02 | | | TC-52.10 | 10/18/13 | | |
| F-3.4 | 7/19/13 | | | | | | | TC-52.20 | 7/18/14 | | |
| | | WO-1.1 | 1/18/13 | | | | | | | | |
| | | WO-1.2 | 1/18/13 | | | | | | | | |

SPECIAL PROVISIONS
WATERWAY PERMIT 1/23/15
GEOTECHNICAL REPORT 6/02/14

FEDERAL PROJECT NO. **E043 (184)**
PID NO. **23436**
CONSTRUCTION PROJECT NO. **NONE**
RAILROAD INVOLVEMENT **NONE**
WYA-30-9.11
1/521



LOCATION MAP

LATITUDE: 41°24'10" LONGITUDE: 82°18'40"



PORTION TO BE IMPROVED - - - - -
 INTERSTATE & DIVIDED HIGHWAY - - - - -
 UNDIVIDED STATE & FEDERAL ROUTES - - - - -
 OTHER ROADS - - - - -

**FOR DESIGN DESIGNATION AND DESIGN
 EXCEPTIONS SEE SHEETS 2 AND 3**

STATE OF OHIO
 DEPARTMENT OF TRANSPORTATION

ERI-2-30.51 AND VARIOUS LOR-2-0.00 AND VARIOUS

**CITY OF VERMILION
 BROWNHELM TOWNSHIP
 VERMILION TOWNSHIP
 ERIE AND LORAIN COUNTIES**

PROJECT DESCRIPTION

REHABILITATION OF 1.72 MILES OF EXISTING PAVEMENT AND SHOULDERS FOR ONGOING RESEARCH PROJECTS INCLUDING THE INSTALLATION OF WEIGH-IN-MOTION INSTRUMENTATION AND THE REHABILITATION OF BRIDGES UNDER WEST REIVER ROAD, VERMILION ROAD, VERMILION INTERCHANGE ROAD, SUNNYSIDE ROAD AND CLAUD ROAD AND OVER THE VERMILION RIVER AND BAUMHART ROAD.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: 9.5 ACRES
 ESTIMATED CONTRACTOR EARTH DISTURBED AREA: 2.6 ACRES
 NOTICE OF INTENT EARTH DISTURBED AREA: 12.1 ACRES

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2013 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY EXCEPT FOR THE RAMPS AND SIDE ROADS AS DESCRIBED ON SHEETS 16 & 17 AND AS SHOWN ON SHEETS 19-22, AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED _____
 DATE _____ DISTRICT DEPUTY DIRECTOR

APPROVED _____
 DATE _____ DIRECTOR, DEPARTMENT OF TRANSPORTATION

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UNDERGROUND UTILITIES

CONTACT BOTH SERVICES TWO WORKING DAYS BEFORE YOU DIG.

Call Before You Dig
1-800-362-2764

(Non-members must be called directly)
 OIL & GAS PRODUCERS
 UNDERGROUND PROTECTION SERVICE
 1-800-925-0988

PLAN PREPARED BY:
 JOHN J. DOE & ASSOC., INC.
 CONSULTING ENGINEERS
 9999 ENGLISH DRIVE
 COMPUTERLAND, OHIO 00000

ENGINEERS SEAL

FOR STRUCTURES
20' & UNDER

SIGNED: *John J. Doe*
 DATE: 11-11-14

ENGINEERS SEAL

FOR ENTIRE PLAN
EXCEPT STRUCTURES
OVER 20'

SIGNED: *Jane C. Smith*
 DATE: 11-11-14

| STANDARD CONSTRUCTION DRAWINGS | | | | | | | | SUPPLEMENTAL SPECIFICATIONS | |
|--------------------------------|---------|----------|---------|----------|---------|----------|----------|-----------------------------|---------|
| BP-1.1 | 7/28/00 | MGS-1.1 | 7/19/13 | HL-10.13 | 1/16/15 | TC-18.24 | 1/17/14 | 800-2015 | 1/21/15 |
| BP-2.1 | 7/19/13 | MGS-2.1 | 7/19/13 | HL-20.14 | 1/16/15 | TC-22.20 | 1/17/14 | 832 | 1/17/14 |
| BP-2.2 | 7/18/08 | MGS-3.1 | 7/18/14 | HL-30.11 | 1/16/15 | TC-41.10 | 7/19/13 | | |
| BP-2.3 | 7/18/14 | MGS-3.2 | 1/18/13 | HL-30.21 | 1/17/14 | TC-41.20 | 10/18/13 | | |
| BP-2.5 | 7/19/13 | MGS-4.2 | 7/19/13 | HL-30.31 | 1/17/14 | TC-41.40 | 10/18/13 | | |
| BP-3.1 | 7/18/14 | MGS-5.3 | 7/19/13 | HL-30.32 | 1/17/14 | TC-41.50 | 10/18/13 | | |
| BP-6.1 | 7/19/13 | MGS-6.1 | 7/19/13 | HL-40.10 | 1/17/14 | TC-42.10 | 10/18/13 | | |
| | | | | | | TC-42.20 | 10/18/13 | | |
| CB-1.1 | 1/18/13 | RM-4.2 | 6/4/14 | MT-95.30 | 7/18/14 | TC-51.11 | 1/17/14 | | |
| CB-2.2 | 1/17/14 | RM-4.3 | 7/18/14 | MT-95.31 | 7/18/14 | TC-52.10 | 10/18/13 | | |
| CB-3.1 | 1/18/13 | RM-4.4 | 7/18/14 | MT-96.11 | 7/18/14 | TC-52.20 | 7/18/14 | | |
| CB-3.2 | 1/18/13 | | | MT-96.20 | 7/19/13 | TC-65.10 | 1/17/14 | | |
| | | | | MT-96.26 | 7/19/13 | TC-65.11 | 7/18/14 | | |
| DM-1.1 | 1/18/13 | AS-1-81 | 1/18/13 | MT-99.20 | 7/19/13 | TC-72.20 | 7/18/14 | | |
| DM-4.4 | 7/20/12 | EXJ-4-87 | 7/19/02 | | | TC-72.20 | 7/18/14 | | |
| | | RB-1-55 | 7/19/13 | | | TC-82.10 | 10/18/13 | | |

SPECIAL PROVISIONS

WATERWAY PERMIT
1/23/14

FEDERAL PROJECT NO. E115 (218)
 CONSTRUCTION PROJECT NO. 20283
 RAILROAD INVOLVEMENT NONE
 ERI-2-30.51 AND VARIOUS LOR-2-0.00 AND VARIOUS
 1/267

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

CRA-C.R. 6-1.61
(BOUNDARY RD.) PART 1
CRANBERRY TOWNSHIP
CRAWFORD COUNTY
FOR PART 2, SEE CAR-C.R. 31 (SCOTT RD.)

PROJECT DESCRIPTION

IMPROVEMENT OF 0.04 MILE OF C.R. 6 (BOUNDARY ROAD) BY REPLACEMENT OF AN EXISTING STEEL TRUSS STRUCTURE OVER BROKEN KNIFE CREEK WITH A PRECAST PRESTRESSED BOX BEAM TYPE STRUCTURE INCLUDING APPROACH RECONSTRUCTION.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: 1.7 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: 0.6 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA: 4.9 ACRES

2013 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT DETOURS WILL BE PROVIDED AS INDICATED ON SHEET 7.

APPROVED _____
DATE _____ CRAWFORD COUNTY COMMISSIONER

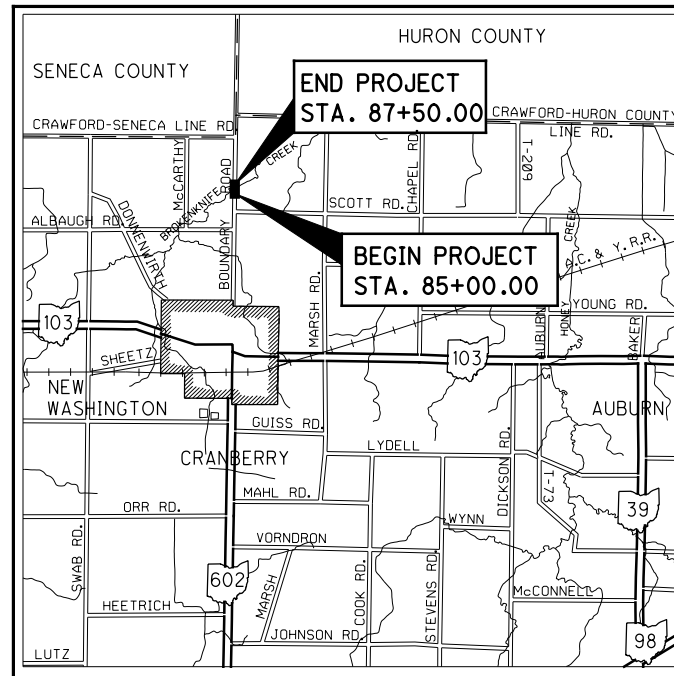
APPROVED _____
DATE _____ CRAWFORD COUNTY COMMISSIONER

APPROVED _____
DATE _____ CRAWFORD COUNTY COMMISSIONER

APPROVED _____
DATE _____ ENGINEER, CRAWFORD COUNTY

APPROVED _____
DATE _____ DISTRICT DEPUTY DIRECTOR

APPROVED _____
DATE _____ DIRECTOR, DEPARTMENT OF TRANSPORTATION



LOCATION MAP

LATITUDE: 40°59'10" LONGITUDE: 82°51'10"



PORTION TO BE IMPROVED - - - - -
INTERSTATE & DIVIDED HIGHWAY - - - - -
UNDIVIDED STATE & FEDERAL ROUTES - - - - -
OTHER ROADS - - - - -

DESIGN DESIGNATION

CURRENT ADT (2013) - - - - - 1500
DESIGN YEAR ADT (2033) - - - - - 2020
DESIGN HOURLY VOLUME (2033) - - - - - 166
DIRECTIONAL DISTRIBUTION - - - - - 50%
TRUCKS (24 HOUR B&C) - - - - - 5%
DESIGN SPEED - - - - - 55 MPH
LEGAL SPEED - - - - - 55 MPH
DESIGN FUNCTIONAL CLASSIFICATION: - - 05 MAJOR COLLECTOR (RURAL)
NHS PROJECT - - - - - NO

DESIGN EXCEPTIONS

NONE REQUIRED

UNDERGROUND UTILITIES
CONTACT BOTH SERVICES TWO WORKING DAYS BEFORE YOU DIG.

OHIO Utilities Protection SERVICE
Call Before You Dig
1-800-362-2764
(Non-members must be called directly)

OIL & GAS PRODUCERS UNDERGROUND PROTECTION SERVICE
1-800-925-0988

PLAN PREPARED BY:

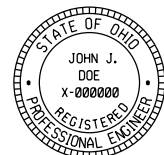
JOHN J. DOE & ASSOC., INC.
CONSULTING ENGINEERS
9999 ENGLISH DRIVE
COMPUTERLAND, OHIO 00000

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| | |
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| CROSS SECTIONS | 14-17 |
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ENGINEERS SEAL:

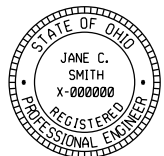
FOR DESIGN CHANGES NOTED ON SHEET 4



SIGNED: John J. Doe
DATE: 11/11/14

ENGINEERS SEAL:

FOR ENTIRE PLAN EXCEPT STRUCTURES 20' & OVER



SIGNED: Jane C. Smith
DATE: 11/11/14

PARTS 1 AND 2

| STANDARD CONSTRUCTION DRAWINGS | | | | | | SUPPLEMENTAL SPECIFICATIONS | |
|--------------------------------|---------|-----------|----------|--|--|-----------------------------|---------|
| BP-3.1 | 7/18/14 | RM-1.1 | 7/18/14 | | | 800-2015 | 1/21/15 |
| CB-1.1 | 1/18/13 | AS-1-81 | 1/18/13 | | | 832 | 1/17/14 |
| CB-1.2 | 1/18/13 | TST-1-99 | 1/17/14 | | | | |
| DM-1.1 | 1/18/13 | MT-105.10 | 7/19/13 | | | | |
| DM-4.4 | 7/20/12 | MT-110.10 | 7/19/13 | | | | |
| MGS-1.1 | 7/19/13 | TC-41.20 | 10/18/13 | | | | |
| MGS-2.1 | 7/19/13 | TC-41.40 | 10/18/13 | | | | |
| MGS-4.2 | 7/19/13 | TC-52.10 | 10/18/13 | | | | |
| MGS-5.3 | 7/19/13 | TC-52.20 | 7/18/14 | | | | |
| HW-2.1 | 1/18/13 | | | | | | |
| HW-2.2 | 1/18/13 | | | | | | |
| | | | | | | SPECIAL PROVISIONS | |
| | | | | | | WATERWAY PERMIT | |
| | | | | | | 1/23/15 | |

FEDERAL PROJECT NO. E017 (212)
CONSTRUCTION PROJECT NO. 24988
RAILROAD INVOLVEMENT NONE
CRA-C.R. 6-1.61 (BOUNDARY RD.) PART 1
1/27

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

CRA-C.R.31

(SCOTT RD.) PART 2

CRANBERRY TOWNSHIP
CRAWFORD COUNTY

FOR PART 1, SEE CRA-C.R. 6-1.61 (BOUNDARY RD.)

PROJECT DESCRIPTION

IMPROVEMENT OF 0.38 MILE OF C.R. 31 (SCOTT RD) BY RECONSTRUCTION OF EXISTING VERTICAL ALIGNMENT TO ELIMINATE HAZARDOUS VERTICAL CURVES INCLUDING UPGRADING OF GUARDRAIL, DRAINAGE, SIGNING AND PAVEMENT MARKINGS.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: 3.5 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: 0.9 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA: 4.9 ACRES

2013 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

APPROVED _____
DATE _____ CRAWFORD COUNTY COMMISSIONER

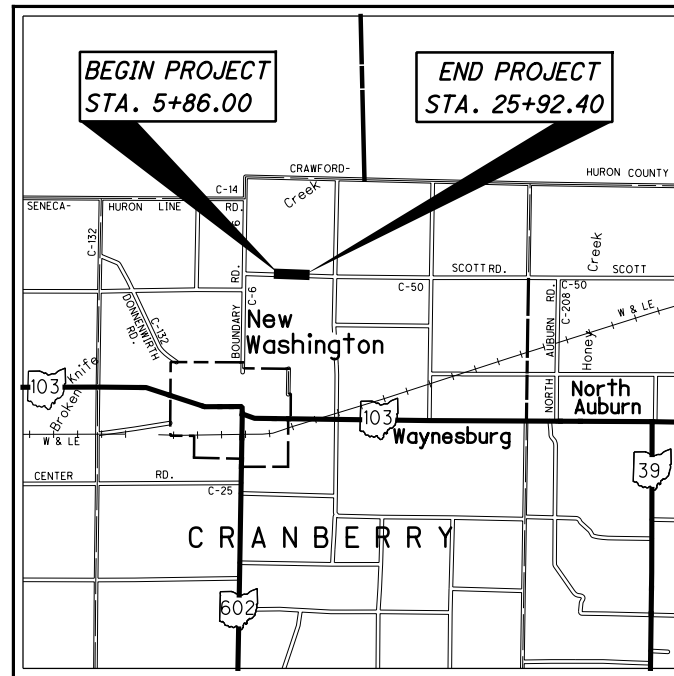
APPROVED _____
DATE _____ CRAWFORD COUNTY COMMISSIONER

APPROVED _____
DATE _____ ENGINEER, CRAWFORD COUNTY

APPROVED _____
DATE _____ DISTRICT DEPUTY DIRECTOR

APPROVED _____
DATE _____ DIRECTOR, DEPARTMENT OF TRANSPORTATION

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT DETOURS WILL BE PROVIDED AS INDICATED ON SHEET 6.



LOCATION MAP

LATITUDE: 40°58'50" LONGITUDE: 82°50'40"



| | |
|----------------------------------|-------|
| PORTION TO BE IMPROVED | ----- |
| INTERSTATE & DIVIDED HIGHWAY | ===== |
| UNDIVIDED STATE & FEDERAL ROUTES | ==== |
| OTHER ROADS | ---- |

DESIGN DESIGNATION

| | | |
|-----------------------------------|---------------------------|--------|
| CURRENT ADT (2013) | ----- | 2300 |
| DESIGN YEAR ADT (2033) | ----- | 3150 |
| DESIGN HOURLY VOLUME (2033) | ----- | 315 |
| DIRECTIONAL DISTRIBUTION | ----- | 55% |
| TRUCKS (24 HOUR B&C) | ----- | 3% |
| DESIGN SPEED | ----- | 55 MPH |
| LEGAL SPEED | ----- | 55 MPH |
| DESIGN FUNCTIONAL CLASSIFICATION: | ___ 07 LOCAL ROAD (RURAL) | |
| NHS PROJECT | ----- | NO |

DESIGN EXCEPTIONS

NONE REQUIRED

UNDERGROUND UTILITIES

CONTACT BOTH SERVICES TWO WORKING DAYS BEFORE YOU DIG.

Call Before You Dig
1-800-362-2764

(Non-members must be called directly)

OIL & GAS PRODUCERS
UNDERGROUND PROTECTION SERVICE
1-800-925-0988

PLAN PREPARED BY:
JOHN J. DOE & ASSOC., INC.
CONSULTING ENGINEERS
9999 ENGLISH DRIVE
COMPUTERLAND, OHIO 00000

ENGINEERS SEAL:

SIGNED: *John J. Doe*
DATE: 11/11/14

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| MAINTENANCE OF TRAFFIC | 6-8 |
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| CALCULATIONS & SUBSUMMARIES | 10 |
| PROJECT SITE PLAN | 11 |
| PLAN & PROFILE | 12-17 |
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| RIGHT OF WAY | 27-35 |
| SOIL PROFILES | |

| STANDARD CONSTRUCTION DRAWINGS | | | | SUPPLEMENTAL SPECIFICATIONS |
|--------------------------------|------------|--|--|-----------------------------|
| | | | | |
| | SEE PART 1 | | | SEE PART 1 |
| | | | | SPECIAL PROVISIONS |
| | | | | SEE PART 1 |

FEDERAL PROJECT NO. **E025 (319)**
PID NO. **24988**
CONSTRUCTION PROJECT NO.
RAILROAD INVOLVEMENT **NONE**
CRA-C.R. 31
(SCOTT RD.) PART 2
1/35

STATE OF OHIO
 DEPARTMENT OF TRANSPORTATION

PROJECT DESCRIPTION

SPOT PAVEMENT REPAIRS ON SR-257 IN DELAWARE COUNTY BETWEEN SLM 8.37 (US-42/SR-745) AND SLM 14.26 (SR-37).
 6' WIDE CONTINUOUS SLOT PAVING ON OUTSIDE EDGE OF SR-257 BEGINNING AT SLM 14.26 (SR-37) IN DELAWARE COUNTY AND ENDING AT SR-47 IN MARION COUNTY (SLM 0.66).

EARTH DISTURBED AREA:

PROJECT EARTH DISTURBED AREA N/A*
 EST. CONTRACTOR EARTH DISTURBED AREA N/A*
 NOTICE OF INTENT EARTH DISTURBED AREA N/A*

* MAINTENANCE PROJECT

FEDERAL PROJECT NO.
NONE

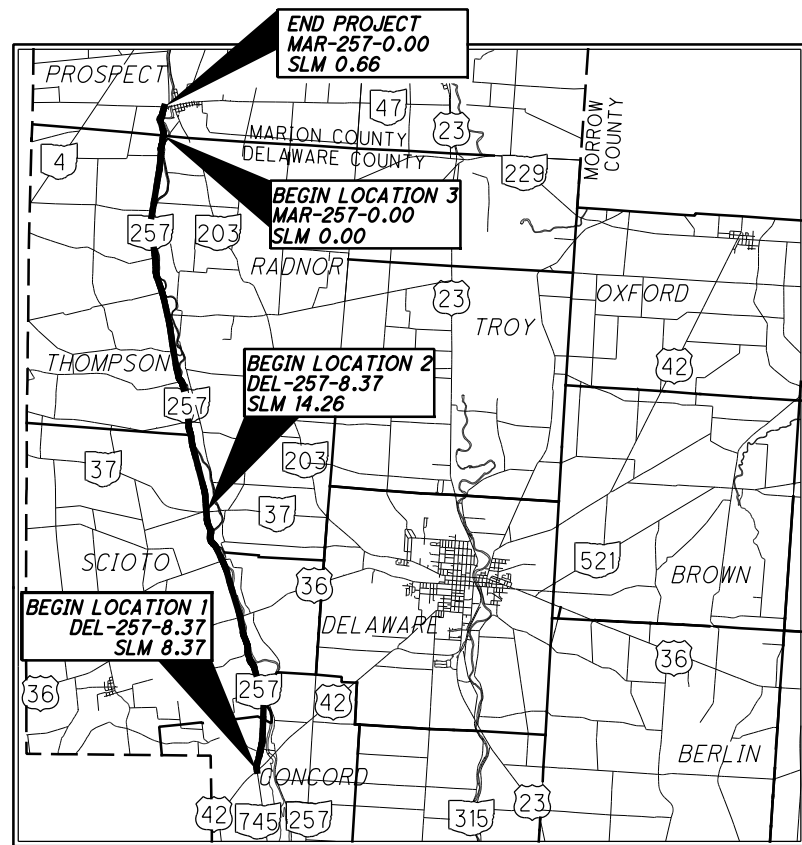
PID NO.
93708

CONSTRUCTION PROJECT NO.

DEL-257-8.37
MAR-257-0.00

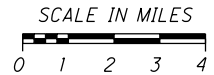
DEL-257-8.37
MAR-257-0.00

VILLAGE OF PROSPECT
CONCORD AND SCIOTO TOWNSHIPS
THOMPSON AND PROSPECT TOWNSHIPS
DELAWARE AND MARION COUNTIES



LOCATION MAP

LATITUDE: 40° 20' 20" LONGITUDE: 83° 10' 47"



| | | |
|-------------------------|-------|-------|
| PORTION TO BE IMPROVED | ----- | ===== |
| INTERSTATE HIGHWAY | ----- | ===== |
| FEDERAL ROUTES | ----- | ===== |
| STATE ROUTES | ----- | ===== |
| COUNTY & TOWNSHIP ROADS | ----- | ===== |
| OTHER ROADS | ----- | ===== |

| DESIGN DESIGNATION | DEL-257-8.37 | MAR-257-0.00 |
|-----------------------------|--------------|--------------|
| CURRENT ADT (2012) | 1,762 | 813 |
| DESIGN YEAR ADT (2024) | 1,869 | 862 |
| DESIGN HOURLY VOLUME (2024) | 78 | 36 |
| DIRECTIONAL DISTRIBUTION | 50% | 50% |
| TRUCKS (24 HOUR B&C) | 4% | 9% |
| DESIGN SPEED | 60 | 60 |
| LEGAL SPEED | 55 | 35/55 |

DESIGN FUNCTIONAL CLASSIFICATION:
 DEL-257-8.37: 05 MAJOR COLLECTOR (RURAL)
 MAR-257-0.00: 06 MINOR COLLECTOR (RURAL)

DESIGN EXCEPTIONS
 NONE REQUIRED

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2013 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

| STANDARD CONSTRUCTION DRAWINGS | | SUPPLEMENTAL SPECIFICATIONS | |
|--------------------------------|----------|-----------------------------|----------|
| BP-3.1 | 7/18/14 | 800-2015 | 1/21/15 |
| | | 821 | 04/20/12 |
| | | 832 | 1/17/14 |
| MT-97.11 | 7/18/14 | | |
| MT-97.12 | 7/18/14 | | |
| MT-99.20 | 7/19/13 | | |
| MT-105.10 | 7/19/13 | | |
| TC-41.20 | 10/18/13 | | |
| TC-42.20 | 10/18/13 | | |
| TC-52.10 | 10/18/13 | | |
| TC-52.20 | 7/18/14 | | |
| TC-65.10 | 1/17/14 | | |
| TC-65.11 | 7/18/14 | | |
| TC-71.10 | 1/17/14 | | |
| TC-82.10 | 10/18/13 | | |

UNDERGROUND UTILITIES
 CONTACT BOTH SERVICES TWO WORKING DAYS BEFORE YOU DIG.

Call Before You Dig
1-800-362-2764

(Non-members must be called directly)

OIL & GAS PRODUCERS
 UNDERGROUND PROTECTION SERVICE
1-800-925-0988

PLAN PREPARED BY:
 JOHN J. DOE & ASSOC., INC.
 CONSULTING ENGINEERS
 9999 ENGLISH DRIVE
 COMPUTERLAND, OHIO 00000

ENGINEERS SEAL

SIGNED: *John J. Doe*
 DATE: 01/21/11

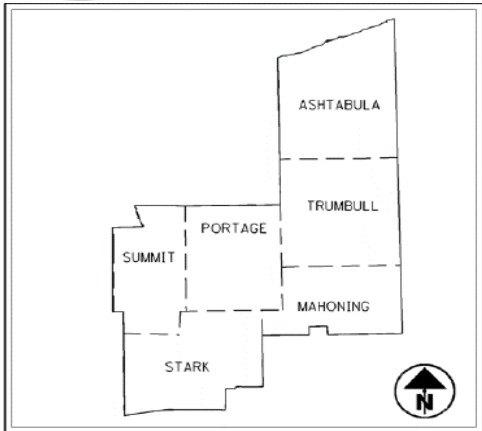
I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED _____
 DATE _____ DISTRICT DEPUTY DIRECTOR

APPROVED _____
 DATE _____ DIRECTOR, DEPARTMENT OF TRANSPORTATION



STATE OF OHIO, DEPARTMENT OF TRANSPORTATION



D04-PMF-FY13

FAST DRY PAVEMENT MARKINGS

CONSTRUCTION PROJECT NUMBER: 13-_____
 PID NO: _____
 FEDERAL PROJECT NUMBER: 100% STATE

PROJECT DESCRIPTION
 PLACEMENT OF PAVEMENT MARKING USING FAST DRY AT EXISTING LOCATIONS.

2013 SPECIFICATIONS
 THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

LIMITED ACCESS:
 THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY THE ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

LATITUDE: 40° 17' 52" LONGITUDE: 83° 02' 58"

Project Earth Disturbed Area: N/A (Maintenance Project)
 Est. Contractor Earth Disturbed Area: N/A (Maintenance Project)
 Notice of Intent Earth Disturbed Area: N/A (Maintenance Project)
 Railroad Involvement: None
 DESIGN DESIGNATION: See Sheet _____
 DESIGN EXCEPTIONS: None

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

UNDERGROUND UTILITIES
 CONTACT BOTH SERVICES TWO WORKING DAYS BEFORE YOU DIG.

OHIO Utilities Protection SERVICE
 (Non-members must be called directly)
 OIL & GAS PRODUCERS UNDERGROUND PROTECTION SERVICE
 1-800-925-0988

Call Before You Dig
 1-800-362-2764

APPROVED: _____
 DISTRICT DEPUTY DIRECTOR

DATE: _____

APPROVED: _____
 DIRECTOR, DEPARTMENT OF TRANSPORTATION

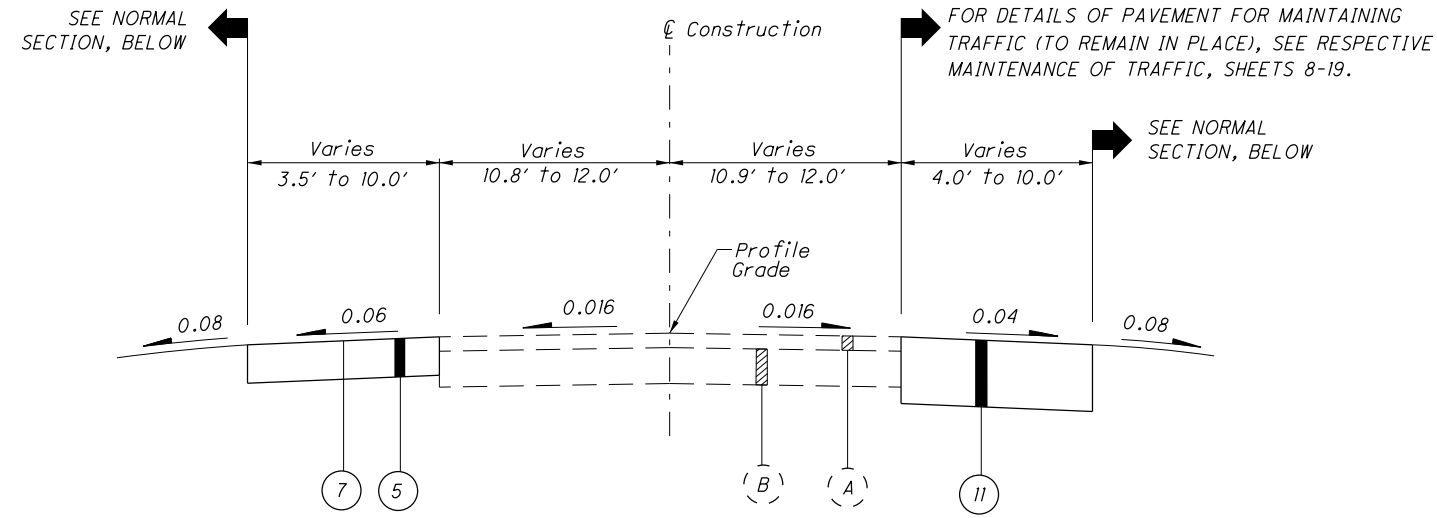
DATE: _____

PLANS PREPARED BY:
 JOHN J. DOE & ASSOCIATES
 CONSULTING ENGINEERS
 999 ENGLISH DRIVE
 COMPUTERLAND, OHIO 00000

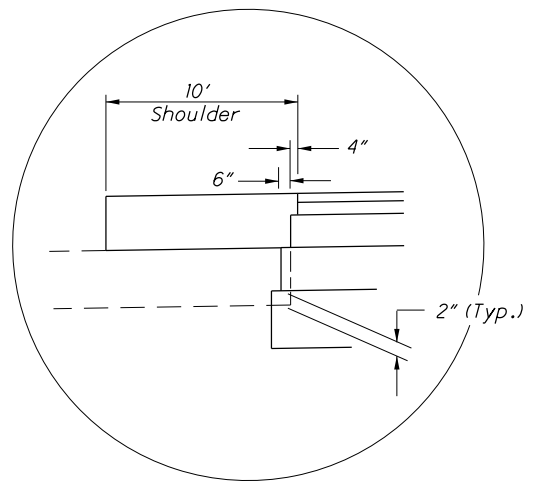
ENGINEERS SEAL:

SIGNED: John Doe
 DATE: 11-11-14

| STANDARD CONSTRUCTION DRAWINGS | | | | | | | | SUPPLEMENTAL SPECIFICATIONS | |
|--------------------------------|----------|-----------|----------|----------|----------|--|--|-----------------------------|-----------|
| MT-95.30 | 07/18/14 | MT-105.10 | 07/19/13 | TC-41.20 | 10/18/13 | | | 800-2015 | 1/15/2015 |
| MT-95.31 | 07/18/14 | | | TC-72.20 | 07/18/14 | | | 832 | 1/17/2014 |
| MT-95.32 | 07/18/14 | | | | | | | | |
| MT-95.50 | 07/19/13 | | | | | | | | |
| MT-98.10 | 07/18/14 | | | | | | | | |
| MT-98.11 | 07/18/14 | | | | | | | | |
| MT-98.20 | 07/18/14 | | | | | | | | |
| MT-98.22 | 07/18/14 | | | | | | | | |
| MT-98.28 | 07/18/14 | | | | | | | | |
| MT-99.20 | 07/19/13 | | | | | | | | |
| | | | | | | | | SPECIAL PROVISIONS | |
| | | | | | | | | NONE | |



NORMAL SECTION - U.S. 46
Sta. 634+00.00 to Sta. 635+75.00

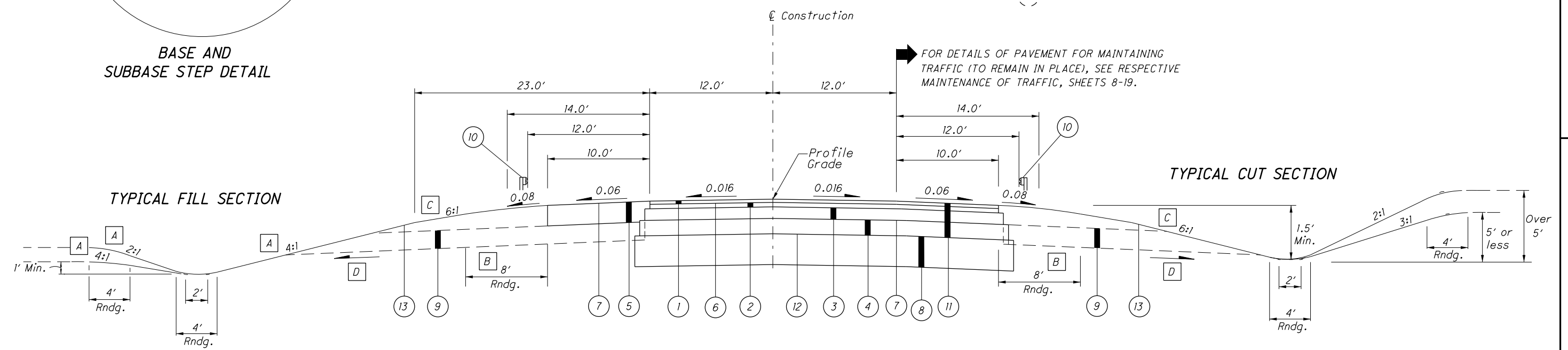


BASE AND SUBBASE STEP DETAIL

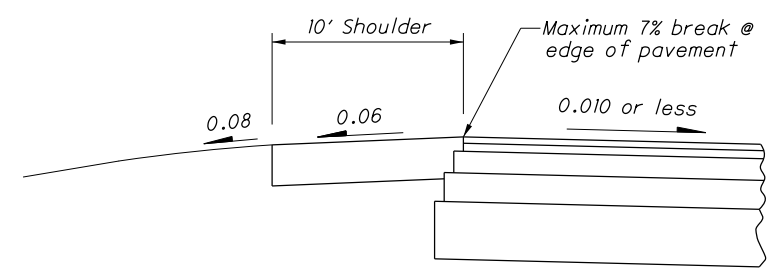
- A** Unless otherwise shown on Cross Sections
- B** No rounding is required when foreslope is 6:1 or flatter. 4' Rounding when guardrail is required.
- C** Foreslope may vary in pavement transition areas at extreme ends of pavement work and adjacent to Structure PIC-46-1209; see cross sections.
- D** 0.04 Min., 0.08 Desirable

LEGEND

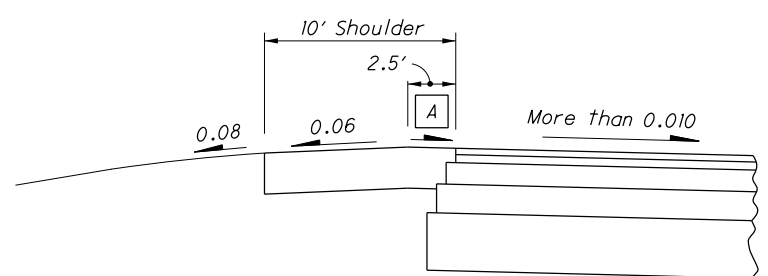
- (1) ITEM 442 - 1½" ASPHALT CONCRETE SURFACE COURSE, 12.5mm, TYPE A (448)
 - (2) ITEM 442 - 1¾" ASPHALT CONCRETE INTERMEDIATE COURSE, 19mm, Type A (448)
 - (3) ITEM 301 - 5" ASPHALT CONCRETE BASE, PG64-22
 - (4) ITEM 304 - 6" AGGREGATE BASE
 - (5) ITEM 304 - 8" AGGREGATE BASE
 - (6) ITEM 407 - TACK COAT
 - (7) ITEM 408 - PRIME COAT (APPLIED AT A RATE OF 0.40 GAL./SQ. YD.)
 - (8) ITEM 206 - LIME STABILIZED SUBGRADE, 18 INCHES DEEP
 - (9) ITEM 605 - AGGREGATE DRAINS
 - (10) ITEM 606 - GUARDRAIL, TYPE 5
 - (11) ITEM 615 - PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN (SEE RESPECTIVE MAINTENANCE OF TRAFFIC DETAILS)
 - (12) ITEM 204 - SUBGRADE COMPACTION
 - (13) ITEM 659 - SEEDING AND MULCHING
 - (14) ITEM 526 - REINFORCED CONCRETE APPROACH SLABS (T=15")
 - (15) NOT USED
 - (16) ITEM 605 - 6" SHALLOW PIPE UNDERDRAINS
- (A) 3" ± ASPHALT CONCRETE
(B) 8" ± CONCRETE PAVEMENT



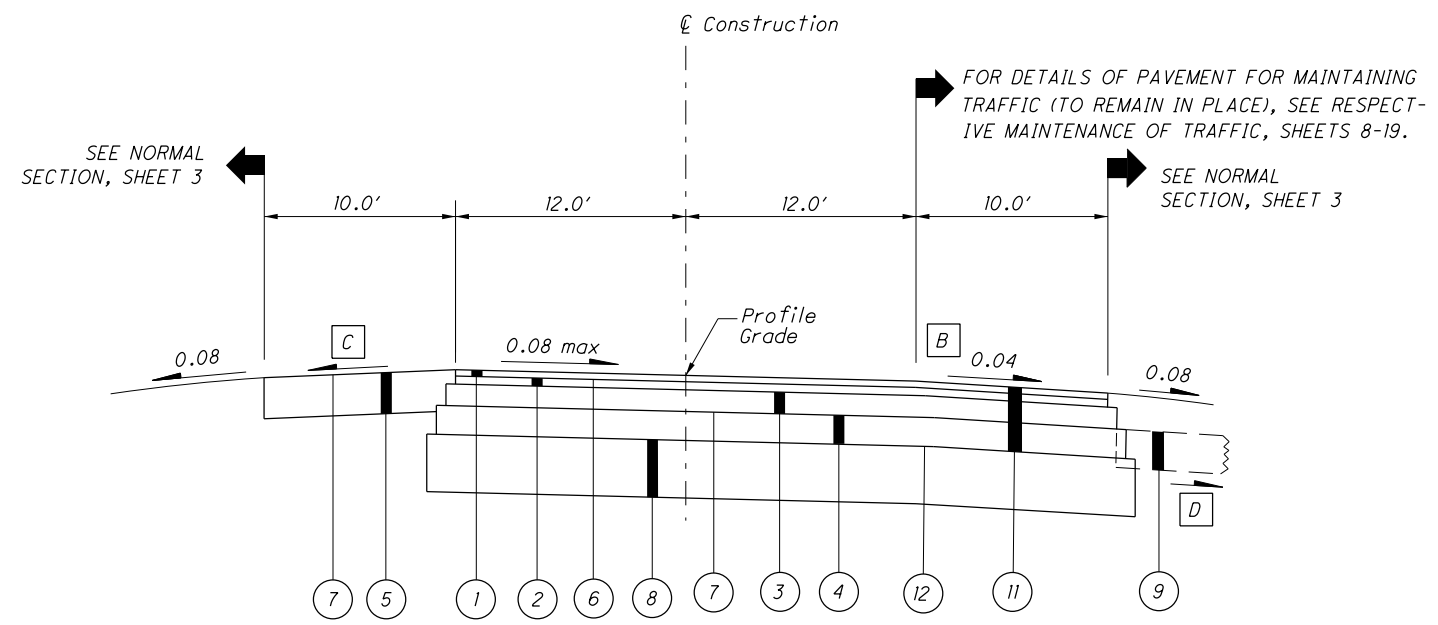
NORMAL SECTION - U.S. 46
Sta. 635+75.00 to Sta. 642+81.37
Sta. 638+22.44 to Sta. 640+48.86



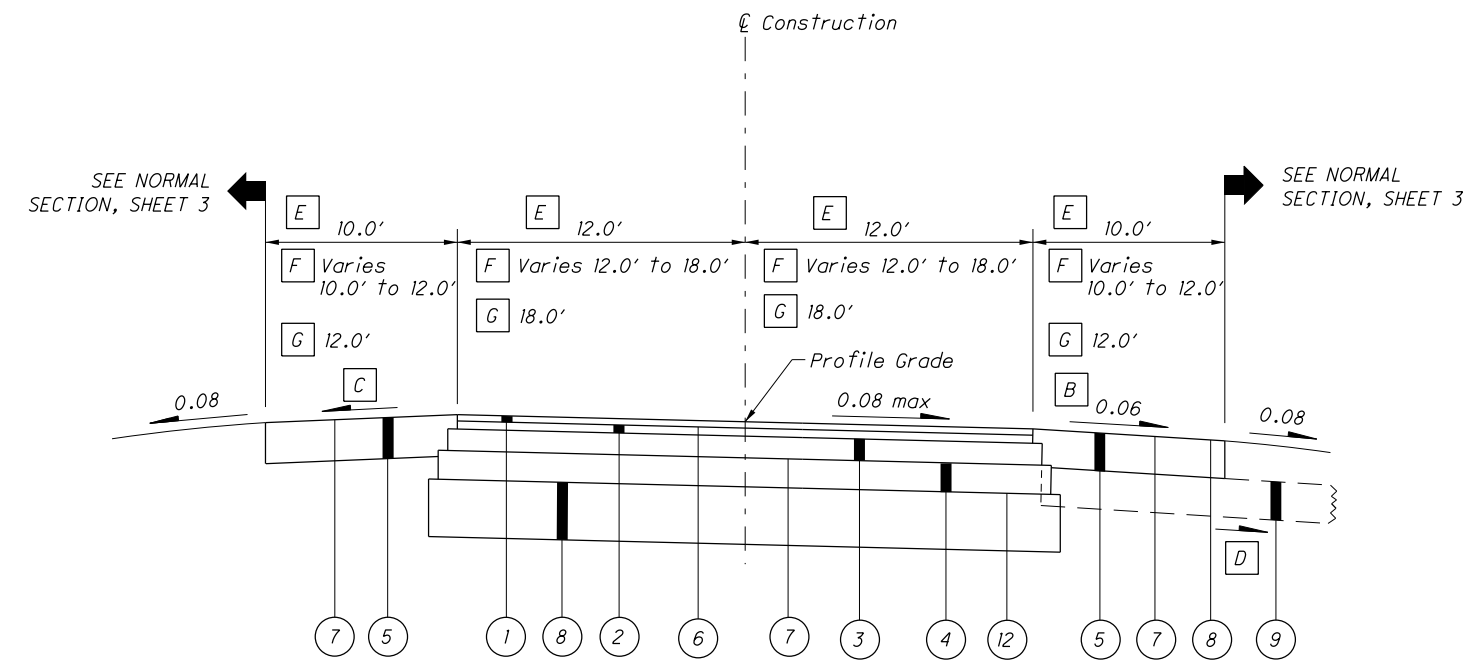
SHOULDER DETAIL
For pavement slopes of 0.010 or less



SHOULDER DETAIL
For pavement slopes of more than 0.010



SUPERELEVATED SECTION - U.S. 46
Sta. 642+81.37 to Sta. 649+00.00

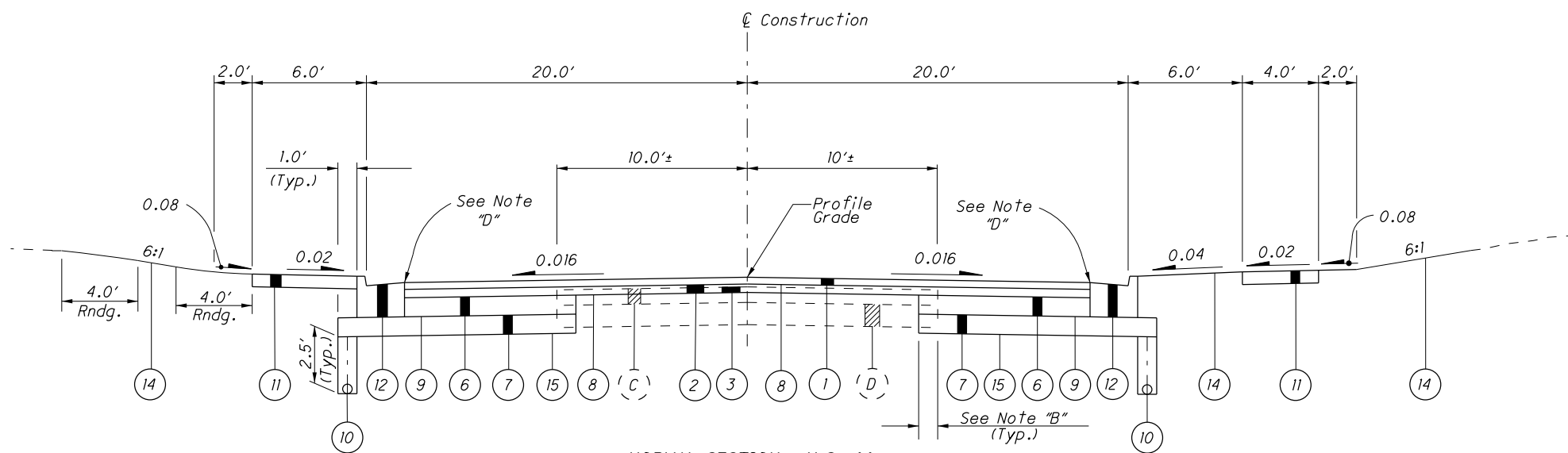
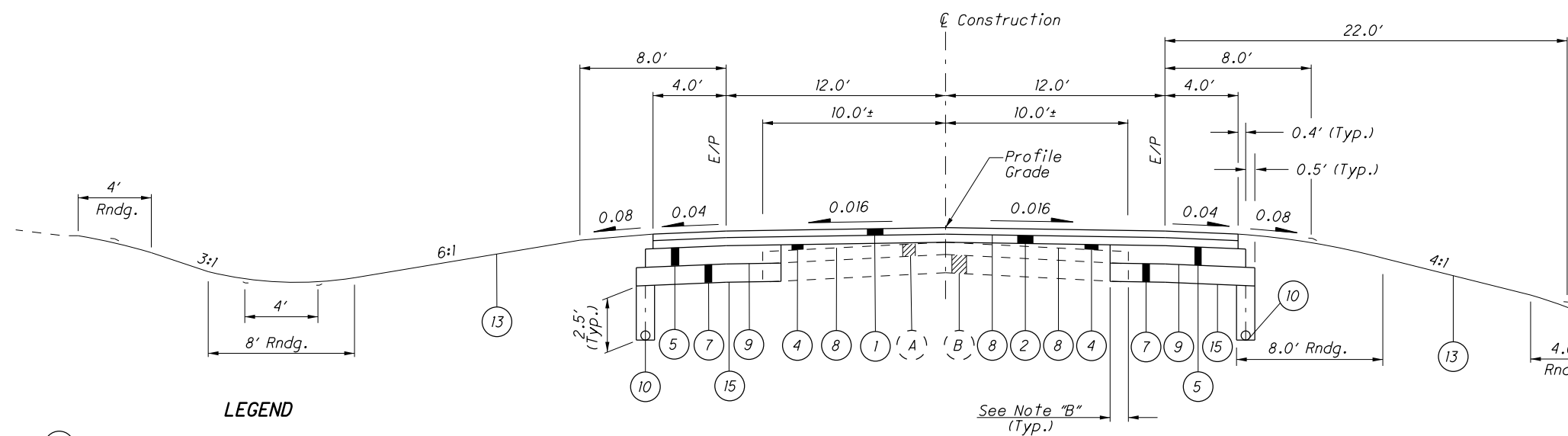
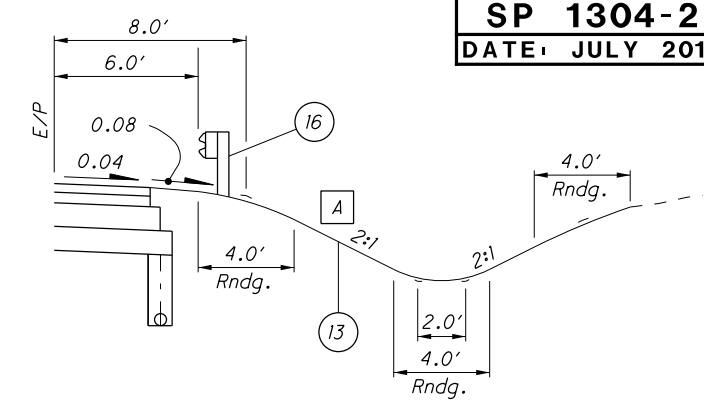


SUPERELEVATED SECTION - U.S. 46

- A** Same slope as pavement
- B** Or pavement slope whichever is greater
- C** For high side shoulder slopes on superelevated sections see shoulder details, this sheet.
- D** 0.04 Min., 0.08 Desirable
- E** Sta. 649+00.00 to Sta. 651+45.03
- F** Sta. 651+45.03 to Sta. 654+75.03
- G** Sta. 654+75.03 to Sta. 658+77.85

FOR DETAILS OF PAVEMENT FOR MAINTAINING TRAFFIC (TO REMAIN IN PLACE), SEE RESPECTIVE MAINTENANCE OF TRAFFIC, SHEETS 8-19.

STA. 656+57.63 TO STA. 658+77.85, SEE INTERSECTION DETAIL ON SHEET 39.
SEE INTERSECTION DETAIL, SHEET 39.
FOR PAVEMENT LEGEND, SEE SHEET 3.
FOR BASE AND SUBBASE STEP DETAIL, SEE SHEET 3.



LEGEND

- ① ITEM 441 - 1/4" Asphalt Concrete Surface Course, Type 1, (448) PG64-22
- ② ITEM 441 - 1 3/4" Asphalt Concrete Intermediate Course, Type 2, (448)
- ③ ITEM 254 - Pavement Planing, Asphalt Concrete (See Note "A")
- ④ ITEM 441 - 0" Min. Asphalt Concrete Intermediate Course, Type 1, (448) (See Note "C")
- ⑤ ITEM 301 - 5" Asphalt Concrete Base, PG64-22
- ⑥ ITEM 301 - 6" Asphalt Concrete Base, PG64-22
- ⑦ ITEM 304 - 6" Aggregate Base
- ⑧ ITEM 407 - Tack Coat
- ⑨ ITEM 408 - Prime Coat (Applied at the Rate of 0.4 gal/sq. yd.)
- ⑩ ITEM 605 - 6" Shallow Pipe Underdrains
- ⑪ ITEM 608 - 4" Concrete Walk
- ⑫ ITEM 609 - Combination Curb and Gutter, Type 2
- ⑬ ITEM 659 - Seeding and Mulching
- ⑭ ITEM 660 - Sodding Unstaked
- ⑮ ITEM 204 - Subgrade Compaction
- ⑯ ITEM 606 - Guardrail, Type 5

- (A) 5± Asphalt Concrete
- (B) 5± Waterbound Macadam
- (C) 4± Asphalt Concrete
- (D) 6± Dense Asphalt Macadam Base

NOTE "A"
Pavement planing shall be a constant depth of 1" at the \hat{C} of construction with a uniform cross slope of 0.016 established. Maximum depth of planing at the outside edges of existing pavement has been calculated to be 2 1/2".

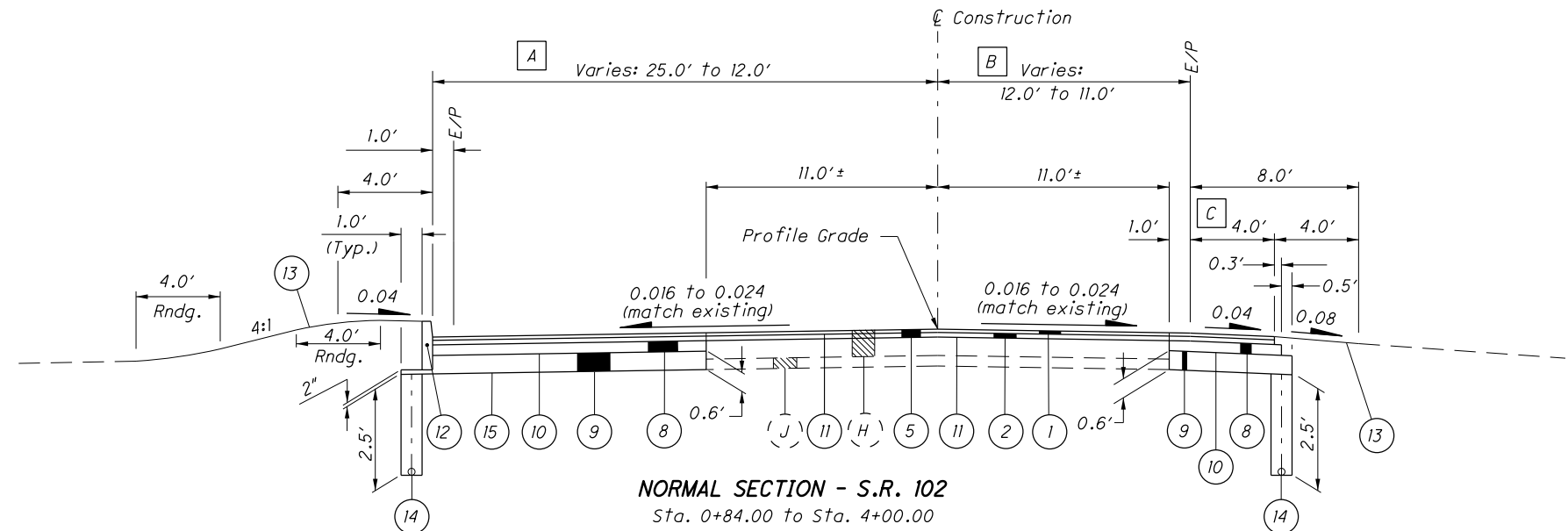
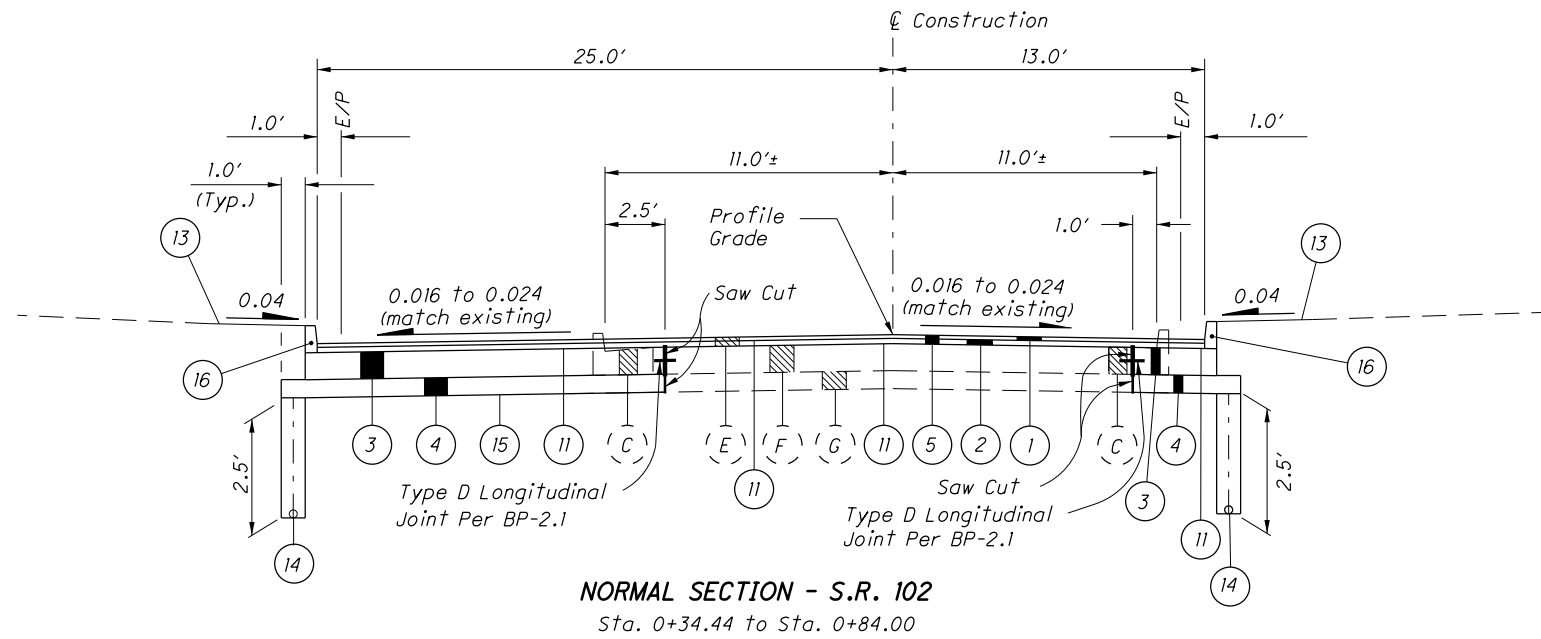
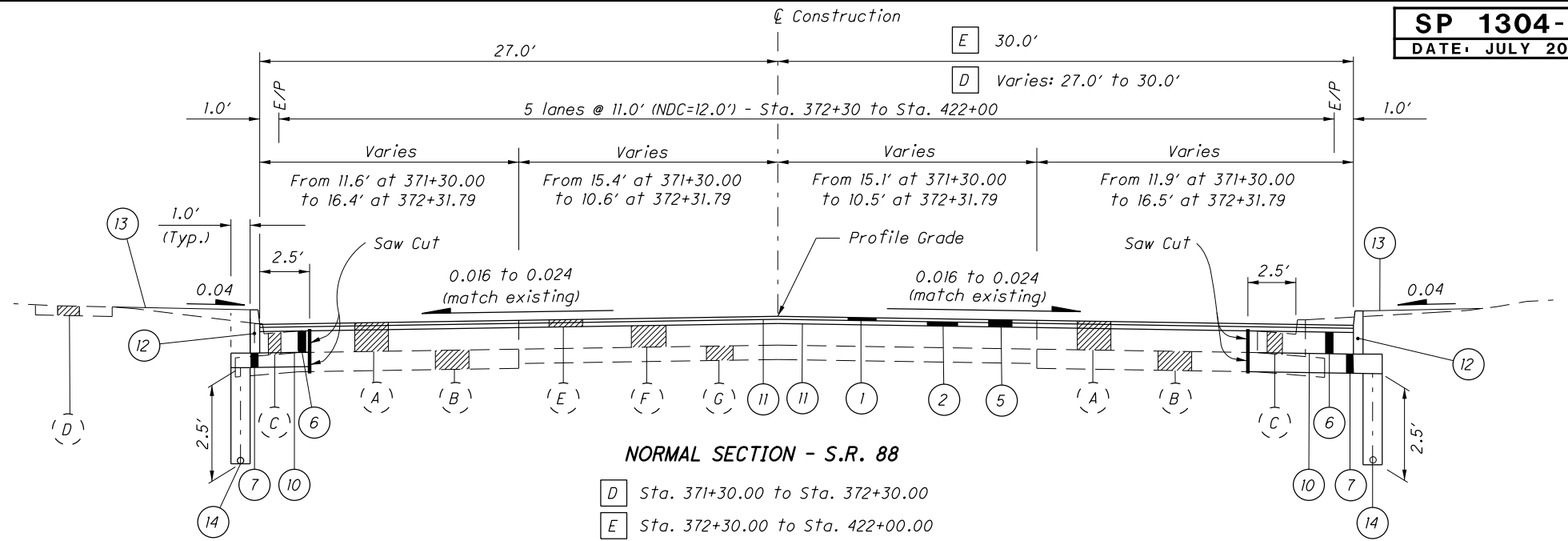
NOTE "B"
The existing pavement edges shall be saw cut to locate a sound pavement edge per sec. 203.04(f) of the CMS. For estimating purposes, pavement calculations included in the plan indicate an average width of 1 ft. of existing pavement being replaced.

NOTE "C"
ITEM 441, Asphalt Concrete Intermediate Course, Type 1, (448), is to be used as a leveling course to establish a 0.016 cross slope. Estimated quantities have been calculated based on exaggerated cross-sections shown on sheets 51-58.

NOTE "D"
ITEM 441, Asphalt Concrete Surface Course, Type 1, (448) PG64-22, is to be 1/4" above gutter plate.

LEGEND

- (1) ITEM 441 - 1 1/4" Asphalt Concrete Surface Course, Type 1, (448) PG64-22
- (2) ITEM 441 - 1 3/4" Asphalt Concrete Intermediate Course, Type 2, (448)
- (3) ITEM 305 - 9" Concrete Base, As Per Plan
- (4) ITEM 304 - 6" Aggregate Base
- (5) ITEM 254 - Pavement Planing, Asphalt Concrete (Max. Depth=3")
- (6) ITEM 301 - 9" Asphalt Concrete Base, PG64-22
- (7) ITEM 304 - 9" Aggregate Base
- (8) ITEM 301 - 4" Asphalt Concrete Base, PG64-22
- (9) ITEM 304 - Aggregate Base, Depth as shown
- (10) ITEM 408 - Prime Coat (Applied At The Rate Of 0.4 Gal/Sq. Yd.)
- (11) ITEM 407 - Tack Coat
- (12) ITEM 609 - Curb, Type 6
- (13) ITEM 660 - Sodding Unstaked
- (14) ITEM 605 - 6" Shallow Pipe Underdrains
- (15) ITEM 204 - Subgrade Compaction
- (16) ITEM 609 - Curb, Type 2-B
- (A) 12" ± Asphalt
- (B) 8" ± Subbase
- (C) Curb & Gutter
- (D) 4" Concrete Sidewalk
- (E) 3" ± Asphalt
- (F) 9" ± Reinforced Concrete
- (G) 6" ± Subbase
- (H) 10" ± Asphalt
- (J) 4" ± Subbase
- [A] 25'-0" From Sta. 0+84 to Sta. 3+50
Varies: 25'-0" @ Sta. 3+50 to 12' @ Sta. 4+00
- [B] 12'-0" From Sta. 0+84 to Sta. 3+50
Varies: 12'-0" @ Sta. 3+50 to 11'-0" @ Sta. 4+00
- [C] Varies: 4' @ Sta. 3+50 to 2' @ Sta. 4+00



S.R. 307 PAVEMENT PLANING TABLES

| STATION | PROPOSED PROFILE ELEVATION | PLANING DEPTH (FEET) @ ϵ | STATION | PROPOSED PROFILE ELEVATION | PLANING DEPTH (FEET) @ ϵ |
|---------|----------------------------|-----------------------------------|---------|----------------------------|-----------------------------------|
| 88+50 | 1094.08 | 0.17 | 92+75 | 1095.26 | 0.27 |
| 88+75 | 1094.17 | 0.15 | 93+00 | 1095.28 | 0.19 |
| 89+00 | 1094.26 | 0.16 | 93+25 | 1095.32 | 0.20 |
| 89+25 | 1094.34 | 0.19 | 93+50 | 1095.36 | 0.21 |
| 89+50 | 1094.43 | 0.20 | 93+75 | 1095.41 | 0.22 |
| 89+75 | 1094.52 | 0.19 | 94+00 | 1095.46 | 0.24 |
| 90+00 | 1094.61 | 0.21 | 94+25 | 1095.51 | 0.28 |
| 90+25 | 1094.69 | 0.20 | 94+50 | 1095.56 | 0.38 |
| 90+50 | 1094.78 | 0.18 | 94+75 | 1095.61 | 0.34 |
| 90+75 | 1094.87 | 0.18 | 95+00 | 1095.66 | 0.26 |
| 91+00 | 1094.96 | 0.18 | 95+25 | 1095.71 | 0.17 |
| 91+25 | 1095.04 | 0.17 | 95+50 | 1095.69 | 0.16 |
| 91+50 | 1095.13 | 0.17 | 95+75 | 1095.67 | 0.15 |
| 91+75 | 1095.16 | 0.18 | 96+00 | 1095.65 | 0.14 |
| 92+00 | 1095.18 | 0.20 | 96+25 | 1095.63 | 0.15 |
| 92+25 | 1095.21 | 0.23 | 96+50 | 1095.61 | 0.16 |
| 92+50 | 1095.23 | 0.28 | | | |

NOTES

- S.R. 307 - THE CROWN SHALL BE WORKED OUT OF THE PAVEMENT BETWEEN STA. 92+57.25 AND STA. 92+97.25.
- THE PAVEMENT BETWEEN STA. 92+97.25 AND STA. 94+02.25 SHALL BE BUILT WITHOUT CROWN.
- THE CROWN SHALL BE WORKED INTO THE PAVEMENT BETWEEN STA. 94+02.25 AND STA. 94+42.25.

A IN AN EFFORT TO REMOVE EXISTING PARABOLIC CROWN AND ESTABLISH A SMOOTH AND CONSISTENT PROFILE THROUGHOUT THE PROJECT, THE PAVEMENT SHALL BE PLANED TO A DEPTH INDICATED IN THE PAVEMENT PLANING TABLE ON THIS SHEET. A 0.016 NORMAL CROSS SLOPE SHALL BE ESTABLISHED FROM THE CENTERLINE TO THE EXISTING EDGE OF PAVEMENT.

B THE PAVEMENT BUILD-UP WHEN ADJOINING AN EXISTING ASPHALT PAVEMENT SHALL BE AS FOLLOWS:

ITEM 441 - 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448) PG64-22

ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)

ITEM 408 - PRIME COAT @ 0.4 GAL./SQ. YD.

ITEM 304 - 8" AGGREGATE BASE

THE PAVEMENT BUILD-UP WHEN ADJOINING AN EXISTING GRAVEL OR SLAG PARKING AREA SHALL BE AS FOLLOWS:

ITEM 304 - 11" AGGREGATE BASE

C UNCLASSIFIED UNDERDRAIN LIMITS:

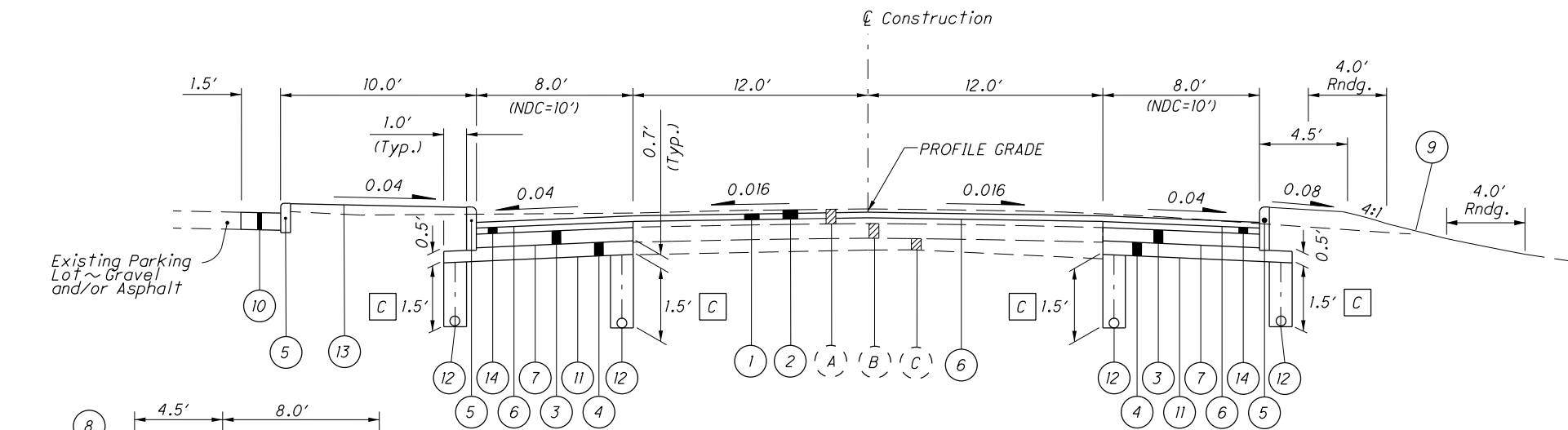
STA. 88+75 TO STA. 91+25 AND

STA. 95+25 TO STA. 96+27.25

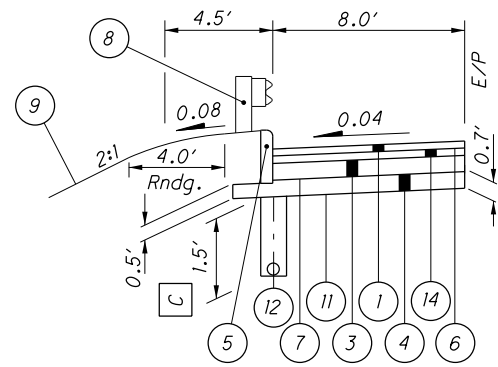
(D) CURB & GUTTER (TO BE REMOVED)

(E) ROADWAY DRAINAGE, 12" (TO BE REMOVED)

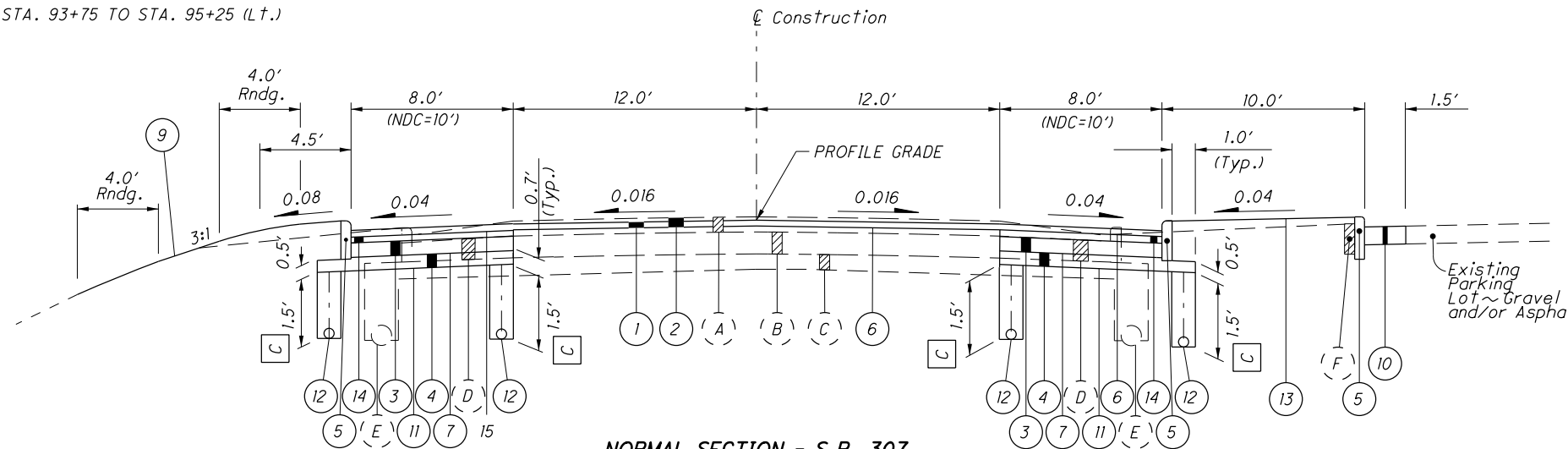
(F) CURB (TO BE REMOVED)



NORMAL SECTION - S.R. 307
STA. 88+75.00 TO STA. 90+15.00



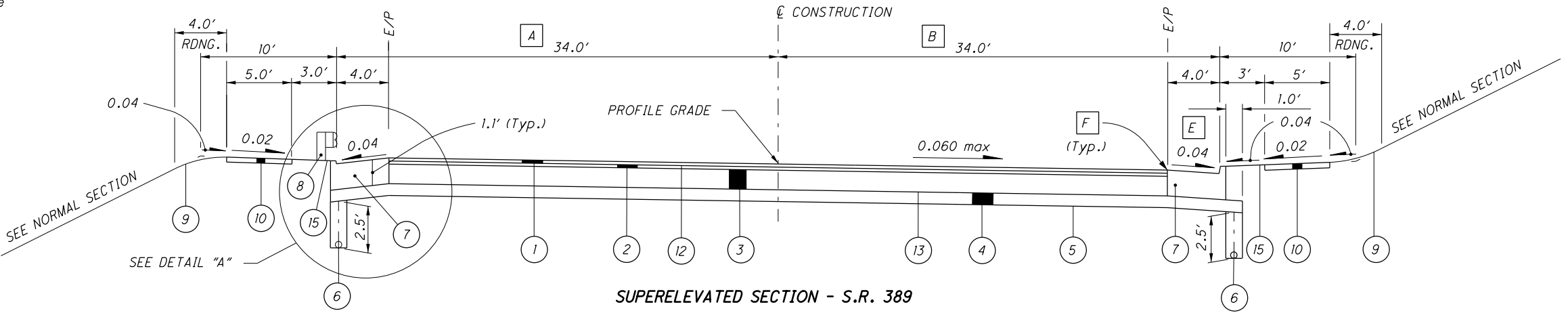
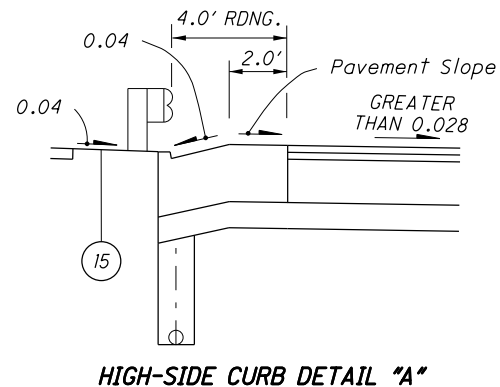
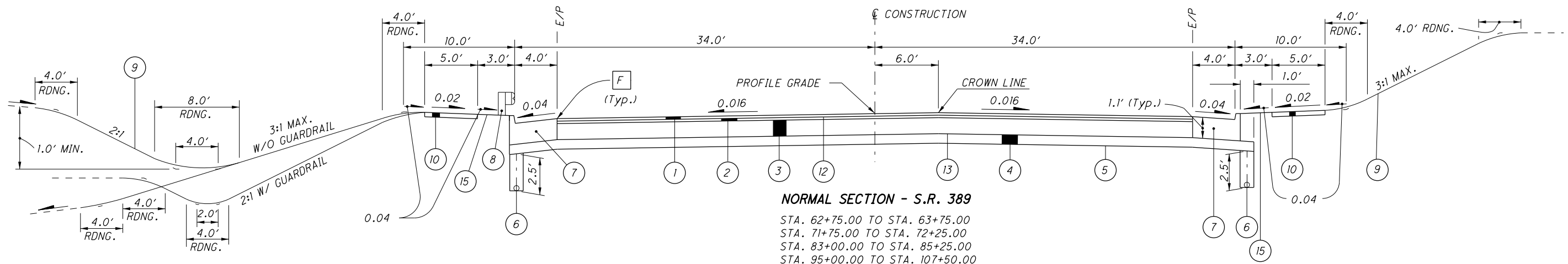
GUARDRAIL SECTION
STA. 93+75 TO STA. 95+25 (L.T.)



NORMAL SECTION - S.R. 307
STA. 90+15.00 TO STA. 96+45.00

LEGEND

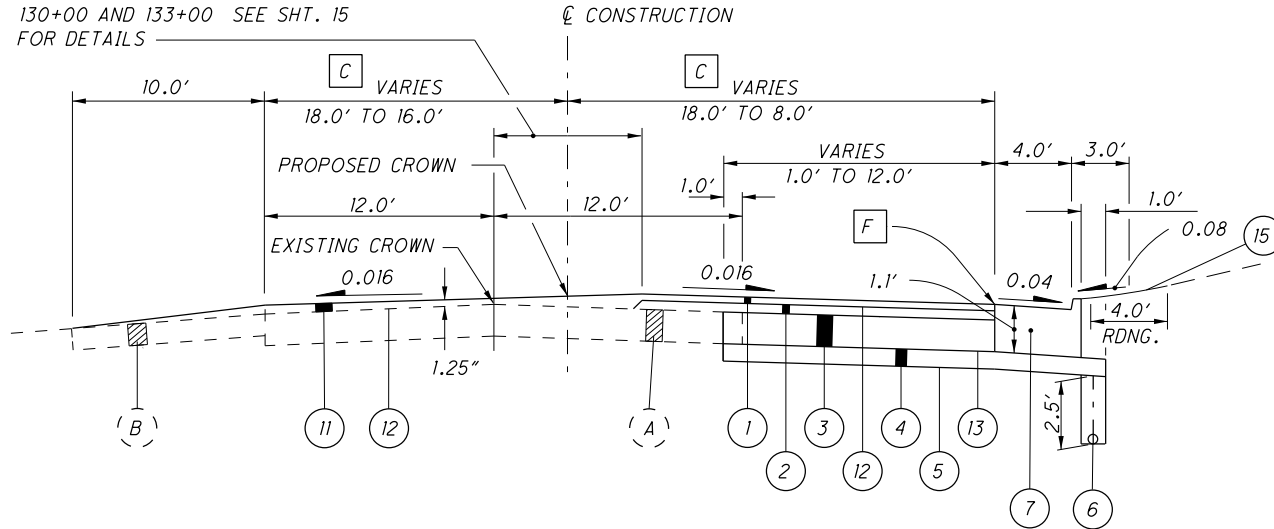
- ① ITEM 441 - 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448) PG64-22
- ② ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE **A**
- ③ ITEM 301 - 9" ASPHALT CONCRETE BASE, PG64-22
- ④ ITEM 304 - AGGREGATE BASE, DEPTH AS SHOWN
- ⑤ ITEM 609 - CURB, TYPE 6
- ⑥ ITEM 407 - TACK COAT
- ⑦ ITEM 408 - PRIME COAT (APPLIED AT THE RATE OF 0.4 GAL./SQ. YD.)
- ⑧ ITEM 606 - GUARDRAIL, TYPE 5
- ⑨ ITEM 659 - SEEDING AND MULCHING (SEE GENERAL NOTE)
- ⑩ SEE PAVEMENT BUILDUP NOTE, THIS SHEET **B**
- ⑪ ITEM 204 - SUBGRADE COMPACTION
- ⑫ ITEM 605 - 6" BASE PIPE UNDERDRAINS
- ⑬ ITEM 660 - SODDING UNSTAKED
- ⑭ ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)
- (A)** 6" ASPHALT SURFACE
- (B)** 9" REINFORCED CONCRETE BASE
- (C)** 6" MIN. CLASSIFIED EMBANKMENT BLANKET COURSE



LEGEND

- ① ITEM 441 - 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446) PG64-22
- ② ITEM 441 - 1 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446)
- ③ ITEM 301 - 10" ASPHALT CONCRETE BASE, PG64-22
- ④ ITEM 304 - 6" AGGREGATE BASE
- ⑤ ITEM 204 - SUBGRADE COMPACTION
- ⑥ ITEM 605 - 6" SHALLOW PIPE UNDERDRAINS [D]
- ⑦ ITEM 609 - COMBINATION CURB AND GUTTER, TYPE 2, AS PER PLAN (SEE SHEET 9)
- ⑧ ITEM 606 - GUARDRAIL, TYPE 5
- ⑨ ITEM 659 - SEEDING AND MULCHING
- ⑩ ITEM 608 - 4" CONCRETE WALK
- ⑪ ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1 (446) PG64-22 (VARIABLE THICKNESS)
- ⑫ ITEM 407 - TACK COAT
- ⑬ ITEM 408 - PRIME COAT (APPLIED AT THE RATE OF 0.4 GAL./SQ. YD.)
- ⑭ NOT USED
- ⑮ ITEM 660 - SODDING UNSTAKED
- (A) 3" ± ASPHALT PAVEMENT OVER 10" ± AGGREGATE SUBBASE
- (B) 8" ± DENSE ASPHALT

TRANSITION PAVEMENT CROWN TO MEET EXISTING CROWN BETWEEN 130+00 AND 133+00 SEE SHT. 15 FOR DETAILS



NOTES:

- FOR VARIABLE PAVEMENT WIDTHS AND SIDEWALK LIMITS, SEE PAVEMENT DETAILS, SHEETS 15 - 18.
- [A] WIDTH VARIES FROM 34'-0" @ STA. 129+50 TO 22'-0" @ STA. 130+00 DUE TO RIGHT TURN LANE TAPER FROM 12' TO 0'
- [B] WIDTH VARIES FROM 34'-0" @ STA. 124+60 TO 22'-0" @ STA. 130+00 DUE TO RIGHT THRU LANE TAPER FROM 12' TO 0'
- [C] PAVEMENT WIDTH VARIES FROM 18'-0", LT. & RT. @ STA. 130+00 TO 16'-0" LT. AND 8'-0", RT. @ STA. 133+00 DUE TO LEFT TURN LANE TRANSITION
- [D] EXCEPT IN AREAS OF GUARDRAIL SECTIONS, PIPE UNDERDRAINS SHALL BE LOCATED IMMEDIATELY BEHIND THE CURB.
- [E] OR PAVEMENT SLOPE, IF GREATER
- [F] ITEM 441, ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446) PG64-22, IS TO BE 1/4" ABOVE GUTTER PLATE.

ITEM 614, MAINTAINING TRAFFIC

THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND DIRECTOR OF PUBLIC WORKS, CITY OF PETERSBURG, AT LEAST 48 HOURS IN ADVANCE (EXCLUSIVE OF SATURDAY, SUNDAY OR HOLIDAYS) OF HIS INTENT TO DIVERT TRAFFIC AND TWO WEEKS IN ADVANCE OF A DETOUR.

NO CHANGE IN TRAFFIC PATTERNS SHALL TAKE PLACE DURING PEAK HOURS, 6:00 A.M. TO 9:00 A.M. AND 3:00 P.M. TO 6:00 P.M. MONDAY THROUGH FRIDAY.

THE CONTRACTOR SHALL NOTIFY CONRAIL (PHONE: 215-596-2923) AND THE ENGINEER AT LEAST ONE WEEK IN ADVANCE OF HIS INTENT TO CLOSE CONRAIL TRACKS TO TRAFFIC FOR THE REMOVAL OF PORTIONS OF EXISTING BRIDGE OVER AND/OR NEAR THE TRACKS. THE TRACK CLOSURES SHALL BE LIMITED TO SATURDAY, SUNDAY AND/OR MONDAY.

ACCESS TO THE PARKING LOT ON BEECHMONT COURT (EAST OF CONRAIL TRACKS) SHALL BE MAINTAINED AT ALL TIMES AND OTHER LOCAL TRAFFIC SHALL BE MAINTAINED AS PER CMS 614.02(A).

S.R. 86 - TWO LANE, TWO WAY TRAFFIC SHALL BE MAINTAINED DURING PEAK HOURS AND AT ALL OTHER TIMES EXCEPT AS FOLLOWS:

ONE LANE, TWO WAY TRAFFIC (USING STANDARD DWG. MT-97.10) WILL BE PERMITTED FOR MINIMUM PERIODS CONSISTENT WITH REQUIREMENTS OF THE SPECIFICATIONS FOR COMPLETED ASPHALT COURSES AND WHEN NECESSARY FOR THE CONTRACTOR'S EQUIPMENT TO OCCUPY THE PAVEMENT FOR A SHORT TIME.

S.R. 86 MAY BE CLOSED TO TRAFFIC UNDER CONDITIONS STATED IN THE SEQUENCE OF CONSTRUCTION.

EASTERN AVENUE AND BEECHMONT CIRCLE MAY BE CLOSED FOR SHORT DURATIONS (15 MINUTES) DURING BRIDGE DEMOLITION OR BRIDGE BEAM ERECTION. TRAFFIC BACKUP SHALL BE CLEARED AFTER EACH CLOSURE AND ALLOWED TO PASS FREELY WITH NO RESTRICTION (ONE LANE IN EACH DIRECTION FOR 10 MINUTES BEFORE ANOTHER CLOSURE IS MADE. TWO LANE, TWO WAY TRAFFIC SHALL BE MAINTAINED DURING PEAK HOURS NOTED ABOVE.

RAMP C AND E MAY BE CLOSED AS PER THE SEQUENCE OF CONSTRUCTION; OTHERWISE, TRAFFIC SHALL BE MAINTAINED ON EXISTING, PAVEMENT FOR MAINTAINING TRAFFIC OR PROPOSED PAVEMENT BASE COURSES.

THE FINAL SURFACE AND INTERMEDIATE PAVEMENT COURSES SHALL BE COMPLETED TO THE EXTENT POSSIBLE DURING THE "FINAL DETOUR" PHASE. THE REMAINING WORK SHALL BE COMPLETED AFTER THE "FINAL DETOUR" PHASE WHILE MAINTAINING TRAFFIC.

BEECHMONT COURT SHALL BE OPEN AT ALL TIMES EXCEPT THAT ACCESS TO #3753 BEECHMONT COURT MAY BE CUT OFF CUT OFF WHEN THE DRIVE TO BEECHMONT CIRCLE IS COMPLETED. ACCESS TO #3755, #3711 BEECHMONT COURT AND #4747 EASTERN AVENUE SHALL BE MAINTAINED AT ALL TIMES.

THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN TRAFFIC THROUGHOUT THE PROJECT CONSTRUCTION FROM BEACHMONT AVENUE/CHURCH PLACE INTERSECTION TO EASTERN AVENUE BY KEEPING THE EXISTING STAIRS LOCATED IN THE NORTHEAST QUADRANT OF THE INTERSECTION OPEN FOR THE DURATION OF THE "INITIAL DETOUR PHASE" AND "PHASE I". THE EXISTING STAIRS SHALL REMAIN OPEN UNTIL PEDESTRIAN ACCESS IS PROVIDED BY OPENING THE PROPOSED STAIRS LOCATED IN THE SOUTHEAST QUADRANT AND CONSTRUCTED DURING PHASE I FOR PEDESTRIAN USE DURING "PHASE II" AND THE "FINAL DETOUR" PHASE.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

SEQUENCE OF CONSTRUCTION

INITIAL DETOUR PHASE

PREPARE TO CLOSE S.R. 86 TO TRAFFIC BY ERECTING TRAFFIC CONTROL (SEE SHEET 32 AND 33) AND COORDINATING THE DETOUR WITH THE CITY OF PETERSBURG. AT THE SAME TIME, SHORE OR BRACE PORTIONS OF THE EXISTING BRIDGE THAT WILL BE USED TO MAINTAIN TRAFFIC. PERFORM ANY OTHER WORK THAT CAN REDUCE THE TIME REQUIRED TO DETOUR TRAFFIC. SEE SHEET NO. 31 FOR DETOUR MAP.

WHEN CITY OF PETERSBURG FORCES HAVE COMPLETED THE DETOUR SIGNING OUTSIDE THE PROJECT AREA AND THE CONTRACTOR HAS COMPLETED ALL DETOUR SIGNING, CLOSURE SIGNING AND BARRIER PLACEMENT WITHIN THE PROJECT AREA, S.R. 86, RAMP C AND RAMP E SHALL BE CLOSED TO TRAFFIC. THIS CLOSURE SHALL BE LIMITED TO 60 CALENDAR DAYS.

WHILE THE HIGHWAY IS CLOSED PRIOR TO "PHASE I", THE FOLLOWING WORK SHALL BE COMPLETED.

- REMOVE THE PLATE GIRDER BRIDGES OVER EASTERN AVENUE AND CONRAIL.

- CONSTRUCT A TEMPORARY BRIDGE AT EACH LOCATION.

- COMPLETE SHORING AND BRACING.

- SAW CUT THE EXISTING CONCRETE BRIDGE SUPERSTRUCTURE AND PIERS AND BEGIN TO REMOVE THE SOUTH PORTION OF THE EXISTING BRIDGE.

- COMPLETE DRIVE TO #3753-55 BEECHMONT COURT.

- CONSTRUCT PAVEMENT FOR MAINTAINING TRAFFIC, AS PER PLAN, AT RAMP C AND E.

- INSTALL THE TEMPORARY TRAFFIC SIGNAL (INCLUDING THE "PREPARE TO STOP WHEN FLASHING" ADVANCE WARNING SIGN AT THE INTERSECTION OF RAMP F/CHURCH AND S.R. 86.

- ERECT TRAFFIC CONTROL AND PORTABLE CONCRETE BARRIER FOR "PHASE I" PRIOR TO OPENING RAMP C. THE SOLID, DOUBLE YELLOW CENTERLINE SHALL BE IN PLACE PRIOR TO PHASE I OPENING TO TRAFFIC.

PHASE I AND II

THE CONTRACTOR IS EXPECTED TO USE ALL MEANS POSSIBLE POSSIBLE INCLUDING, BUT NOT RESTRICTED TO, MULTIPLE SHIFTS, TWENTY-FOUR (24) HOURS PER DAY SCHEDULING SEVEN (7) DAYS A WEEK (SUBJECT TO THE RESTRICTIONS OF SECTION 910.8 OF THE CITY OF PETERSBURG MUNICIPAL CODE GOVERN-

ING NIGHTTIME CONSTRUCTION BETWEEN THE HOURS OF 11:00 P.M. AND 7:00 A.M.), ADDITIONAL CREWS, LIGHTING FOR NIGHT WORK, MULTIPLE MATERIAL SOURCES, MULTIPLE SUBCONTRACTORS, ETC., IN ORDER TO COMPLETE PHASE I AND II WITHIN 120 CALENDAR DAYS. NO TIME EXTENSIONS (SEE PROPOSAL NOTE) OF THIS INTERIM COMPLETION PERIOD WILL BE CONSIDERED. FAILURE TO OPEN THE HIGHWAY TO FOUR LANE TRAFFIC WILL RESULT IN THE ASSESSMENT OF \$15,000.00 LIQUIDATED DAMAGES FOR EACH CALENDAR DAY (INCLUDING WEEKENDS AND HOLIDAYS) BEYOND THE ALLOTTED TIME.

PHASE I

AFTER THE INITIAL PHASE DETOUR WORK IS COMPLETED, RE-OPEN S.R. 86 AND RAMPS C AND E TO TRAFFIC USING THE TEMPORARY BRIDGES AND A PORTION OF THE EXISTING BRIDGE TO MAINTAIN ONE LANE OF TRAFFIC IN EACH DIRECTION.

COVER DETOUR SIGNS FOR RE-USE DURING THE "FINAL DETOUR" PHASE.

COMPLETE CONSTRUCTION OF THE SOUTH ONE HALF OF THE PROPOSED BRIDGE, RETAINING WALLS, TEMPORARY RETAINING WALLS AND STAIRS IN THE SE QUADRANT OF S.R. 86 AND CHURCH PLACE/RAMP F INTERSECTION.

ERECT TRAFFIC CONTROL AND PORTABLE CONCRETE BARRIER, AND ADJUST TEMPORARY TRAFFIC SIGNAL FOR "PHASE II". THE SOLID, DOUBLE YELLOW CENTERLINE SHALL BE IN PLACE PRIOR TO "PHASE II" OPENING TO TRAFFIC.

PHASE II

AFTER PHASE I WORK IS COMPLETED, RELOCATE TRAFFIC ON S.R. 86 AND RAMPS C AND E FOR "PHASE II" USING THE COMPLETED PORTION OF THE NEW STRUCTURE (MAINTAINING ONE LANE OF TRAFFIC IN EACH DIRECTION) AND NEW FULL DEPTH BASE COURSES ON THE RAMPS.

REMOVE THE TEMPORARY BRIDGES AND THE BALANCE OF THE EXISTING BRIDGE. COMPLETE CONSTRUCTION (EXCEPT THE GAP (SECTION OF DECK).

COMPLETE WORK ON BEECHMONT COURT.

SOME ITEMS (I.E. SANITARY) ARE NOT INCLUDED IN THE SEQUENCE, BECAUSE THEY HAVE ONLY MINOR EFFECT ON MAINTAINING TRAFFIC. THE CONTRACTOR MAY COMPLETE THIS WORK WHEN CONVENIENT DURING THE SEQUENCE OF CONSTRUCTION.

FINAL DETOUR PHASE

UNCOVER DETOUR SIGNS, SET UP CLOSURE SIGNING AND PLACE BARRIER TO CLOSE S.R. 86 AND RAMP C TO TRAFFIC. COORDINATE THE CLOSURE WITH THE CITY OF PETERSBURG, AS BEFORE. SEE SHEET 31 FOR DETOUR MAP.

WHILE THE HIGHWAY IS CLOSED, THE FOLLOWING WORK SHALL BE CONSTRUCTED:

- CLOSE THE REMAINING GAP IN THE DECK NOT COMPLETED IN "PHASE I AND II".

- COMPLETE THE PLACEMENT OF FULL DEPTH PAVEMENT BASE COURSES.

REMOVE TRAFFIC SIGNAL FOR MAINTAINING TRAFFIC.

THIS CLOSURE WILL BE LIMITED TO FIVE DAYS, TWO OF WHICH SHALL BE SATURDAY AND SUNDAY.

FAILURE TO RE-OPEN ON TIME WILL RESULT IN THE ASSESSMENT OF \$25,000.00 LIQUIDATED DAMAGES FOR EACH CALENDAR DAY (INCLUDING WEEKENDS AND HOLIDAYS) BEYOND THE ALLOTTED TIME.

PRIOR TO OPENING THE PROJECT TO TRAFFIC, THE SOLID, DOUBLE YELLOW CENTERLINE SHALL BE IN PLACE AND MAINTAINED DURING SURFACE AND INTERMEDIATE PAVEMENT COURSE PLACEMENT OPERATIONS NOT COMPLETED IN THE "FINAL DETOUR" PHASE.

AFTER THE "FINAL DETOUR" PHASE, IT MAY BE NECESSARY TO REDUCE THE NUMBER OF LANES TO LESS THAN FOUR IN ORDER TO COMPLETE THE PROJECT. THIS WILL BE ACCEPTABLE BUT ONLY DURING ACTUAL CONTRACTOR WORKING HOURS WITH TRAFFIC CONTROL PER APPROPRIATE STANDARD DRAWINGS. NO REDUCTION IN THE NUMBER OF THE LANES SHALL BE PERMITTED DURING PEAK HOURS, THAT BEING FROM 6:00 A.M. TO 9:00 A.M. AND FROM 3:00 P.M. TO 6:00 P.M.

MAINTENANCE OF TRAFFIC GENERAL NOTES

FAY-86-7.39

SEQUENCE OF CONSTRUCTION

IT IS THE INTENT OF THE FOLLOWING SEQUENCE OF CONSTRUCTION TO PROVIDE A WORK AREA FOR THE CONTRACTOR WHILE ALSO MAINTAINING TRAFFIC IN A MANNER WHICH IS SAFE FOR THE TRAVELING PUBLIC; THEREFORE, ALL PHASES SHALL HAVE STRICT ADHERENCE.

ALL TEMPORARY OR PERMANENT PAVEMENT MARKINGS SHALL BE IN PLACE BEFORE ANY PAVEMENT IS OPENED TO TRAFFIC.

PHASE ONE

THE CONTRACTOR SHALL REPLACE THE OUTSIDE BERM WITH AN 8' SHOULDER IN THE DESIGNATED AREAS WITH TEMPORARY PAVEMENT USING A ONE LANE CLOSURE PER MT-95.30. LANE CLOSURE PER MT-95.30.

AREAS OF SHOULDER REPLACEMENT:

| EASTBOUND | WESTBOUND |
|---------------------|---------------------|
| 50+49 to 51+28.25 | 1833+00 to 8+86 |
| 55+81.25 to 58+20 | 48+90 to 51+28.25 |
| 116+23 to 128+26 | 55+81.25 to 60+36 |
| 150+12 to 160+10 | 72+48 to 88+20.5 |
| 167+67 to 175+76.71 | 95+78 to 102+82 |
| 177+35.21 to 183+56 | 112+72 TO 126+26 |
| | 136+82 to 144+13 |
| | 172+63 to 175+76.71 |
| | 177+35.21 to 180+55 |

AFTER THE SHOULDER REPLACEMENT WORK IS COMPLETED, THE CONTRACTOR SHALL THEN PERFORM THE JOINT REPAIRS IN THE FOLLOWING AREAS:

EASTBOUND AREA

| | |
|---|------------------------------|
| A | STA. 147+97 TO STA. 150+05 |
| B | STA. 113+12 TO STA. 115+94 |
| C | STA. 58+26 TO STA. 63+00 |
| D | STA. 1828+26 TO STA. 1832+61 |

WESTBOUND AREA

| | |
|---|----------------------------|
| E | STA. 103+30 TO STA. 107+08 |
| F | STA. 109+40 TO STA. 112+44 |
| G | STA. 129+43 TO STA. 136+63 |
| H | STA. 144+25 TO STA. 147+82 |

THE JOINT REPAIRS SHALL BE PERFORMED IN ALPHABETICAL ORDER ON EACH SIDE AND THE PAVEMENT WILL BE OPEN TO TRAFFIC AS SPECIFIED IN THE PLAN NOTE.

FOR ADDITIONAL PHASE I DETAILS AND QUANTITIES SEE SHEETS 22-23.

PHASE TWO

THE CONTRACTOR SHALL PERFORM THE WORK ON THE INSIDE LANES, WHICH SHALL INCLUDE THE JOINT REPAIR, FULL-DEPTH PAVEMENT, BERM REPLACEMENT, AND BRIDGE REHABILITATION. THE JOINT REPAIRS SHALL BE DONE PRIOR TO THE BERM REPLACEMENT. TRAFFIC SHALL BE MAINTAINED DURING THIS PHASE PER THE DETAILS SHOWN ON SHEETS 24 THRU 37, EXCEPT THAT

CORES WILL BE TAKEN DURING THIS PHASE WHICH WILL REQUIRE THE CLOSING OF BOTH LANES FOR A BRIEF PERIOD. ODOT WILL PROVIDE TRAFFIC MAINTENANCE FOR THE CORING PRO-

PHASE THREE

THE CONTRACTOR SHALL PERFORM THE WORK ON THE OUTSIDE LANES, WHICH SHALL INCLUDE THE JOINT REPAIR, FULL-DEPTH PAVEMENT, BERM REPLACEMENT, AND BRIDGE REHABILITATION. THE JOINT REPAIRS SHALL BE DONE PRIOR TO THE BERM REPLACEMENT. TRAFFIC MAINTENANCE DETAILS FOR THIS PHASE ARE SHOWN ON SHEETS 38 THRU 52.

PHASE FOUR

THE CONTRACTOR SHALL GRIND AND SEAL THE PAVEMENT MAINTAINING TRAFFIC BY USE OF A ONE-LANE CLOSURE PER STANDARD DRAWING MT-95.30. THIS WORK SHALL BE PERFORMED ON BOTH LANES AND IN BOTH DIRECTIONS.

BRIDGES

WEST RIVER ROAD AND VERMILION ROAD BRIDGES WILL BE CONSTRUCTED PART-WIDTH USING A TEMPORARY SIGNAL INSTALLATION TO MAINTAIN ONE LANE, TWO-WAY TRAFFIC. DETAILS FOR MAINTAINING TRAFFIC AT THESE BRIDGES ARE SHOWN ON SHEETS 53 AND 54. SUNNYSIDE ROAD AND CLAUS ROAD BRIDGES MAY BE CLOSED FOR A MAXIMUM OF 30 DAYS EACH. BUT THEY SHALL NOT BE CLOSED AT THE SAME TIME. THE DETOUR PLAN FOR THESE BRIDGES IS SHOWN ON SHEETS 19 AND 20. DETAILS FOR THE VERMILION INTERCHANGE BRIDGE CLOSURE ARE SHOWN ON SHEET 21.

SIDE ROAD STRUCTURES OVER FREEWAY

FOUR LANE, TWO WAY TRAFFIC ON THE FREEWAY SHALL BE MAINTAINED AT ALL TIMES DURING THE REHABILITATION OF THE EXISTING STRUCTURES OVER THE FREEWAY, EXCEPT AS FOLLOWS:

1. DURING THE RETROFITTING OF THE EXISTING OVERHEAD PARAPETS.
2. UNLESS OTHERWISE SHOWN IN THE PLAN.

A SAFETY NET OR PLATFORM SHALL BE REQUIRED TO PROTECT TO THE TRAVEL LANES OF THE FREEWAY DURING RETROFITTING OF EXISTING CONCRETE PARAPETS. THE DESIGN OF THE NET OR PLATFORM SHALL CONFORM WITH OSHA REQUIREMENTS, SHALL HAVE APPROVAL FROM THE ODOT OFFICE OF STRUCTURAL ENGINEERING, AND SHALL REMAIN IN PLACE UNTIL WORK HAS BEEN COMPLETED. THE EXISTING VERTICAL CLEARANCE OVER THE FREEWAY SHALL BE MAINTAINED AT ALL TIMES.

IN THE EVENT A LANE RESTRICTION ON THE FREEWAY IS NECESSARY, THE METHOD OF INSTALLATION AND DESIGN OF TEMPORARY AND DESIGN OF TEMPORARY LANE CLOSURE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING MT-95.30. COST FOR THE ABOVE WORK SHALL BE CONSIDERED INCIDENTAL AND SHALL BE INCLUDED IN ITEM 614, MAINTAINING TRAFFIC.

FREEWAY STRUCTURES OVER SIDE ROADS

TWO LANE, TWO WAY TRAFFIC ON SIDE ROADS SHALL BE MAINTAINED AT ALL TIMES DURING REPLACEMENT OF BEARINGS AND REHABILITATION OF MAINLINE BRIDGES, EXCEPT DURING THE FOLLOWING OPERATIONS:

- 1.) DEMOLITION OF THE EXISTING BRIDGE PARAPETS.
- 2.) CONSTRUCTION OF THE PROPOSED PARAPET OVER THE LOCAL ROAD OR STATE ROUTE WHERE THE ENGINEER BELIEVES TEMPORARY CLOSURE OF A TRAFFIC LANE IS WARRANTED.

A SAFETY NET OR PLATFORM SHALL BE REQUIRED TO PROTECT THE UNDERPASS ROADWAY DURING REMOVAL OF EXISTING AND CONSTRUCTION OF NEW CONCRETE PARAPETS. THE DESIGN OF THE NET OR PLATFORM SHALL CONFORM WITH OSHA REQUIREMENTS, SHALL HAVE APPROVAL FROM THE ODOT OFFICE OF STRUCTURAL ENGINEERING, AND SHALL REMAIN IN PLACE UNTIL WORK HAS BEEN COMPLETED. THE EXISTING VERTICAL CLEARANCE

OVER THE UNDERPASS ROADWAY SHALL BE MAINTAINED AT ALL TIMES. IN THE EVENT A LANE RESTRICTION IS NECESSARY, THE METHOD OF INSTALLATION AND DESIGN OF THE TEMPORARY LANE CLOSURE SHALL CONFORM TO STANDARD DRAWINGS MT-95.30 OR MT-97.10. COST FOR THE ABOVE WORK SHALL BE CONSIDERED INCIDENTAL AND INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC.

PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN

THE PAVEMENT BUILDUP SHALL BE:

- 6" - ITEM 301, ASPHALT CONCRETE BASE, PG64-22
- 4" - ITEM 304, AGGREGATE BASE

PAYMENT SHALL INCLUDE ANY ADDITIONAL COST OF ITEM 203, EXCAVATION TO PLACE THE ITEM 301 OR ITEM 304. THE PAVEMENT FOR MAINTAINING TRAFFIC SHALL BE REMOVED UNDER ITEM 203.

PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B, AS PER PLAN

THE PAVEMENT BUILDUP SHALL BE:

- 6" - ITEM 301, ASPHALT CONCRETE BASE, PG64-22
- 4" - ITEM 304, AGGREGATE BASE.

PAYMENT SHALL INCLUDE ANY ADDITIONAL COST OF ITEM 203, EXCAVATION TO PLACE THE ITEM 301 OR ITEM 304. THE SUBGRADE SHALL BE COMPACTED TO A DEPTH OF 12" ACCORDING TO THE CONSTRUCTION AND MATERIALS SPECIFICATION, SECTION 204.03 AND PAYMENT FOR SUCH WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 204, SUBGRADE COMPACTION (SEE SHEETS 148 THRU 151 FOR QUANTITIES). THIS PAVEMENT SHALL REMAIN IN PLACE.

NOTICE OF CLOSURE SIGNS

THESE SIGNS SHALL BE ERECTED BY THE CONTRACTOR AT LEAST ONE WEEK IN ADVANCE OF THE SCHEDULED ROAD OR RAMP CLOSURE. THE SIGNS SHALL BE ERECTED ON THE RIGHT HAND BESIDE OF THE ROAD/RAMP FACING TRAFFIC AND SHALL BE LOCATED IN THE FIELD SO AS NOT TO INTERFERE WITH ANY

PERMANENT SIGNS. THE SIGNS SHOULD BE ERECTED ALONG ROADWAYS AT THE POINT OF CLOSURE. THE SIGNS MAY BE ANYWHERE ALONG RAMPS WHERE THEY ARE VISIBLE TO THE MOTORIST USING THE RAMP, EXCEPT THAT ON ENTRANCE RAMPS, THE SIGNS SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTION TO THE MOTORIST.

PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 MAINTAINING TRAFFIC AND SHALL INCLUDE FURNISHING, ERECTING, MAINTAINING AND REMOVING THE SIGNS, INCLUDING SUPPORTS.

WILL BE
CLOSED
FOR _____ DAYS
OHIO DEPT. OF TRANSPORTATION

W20-H14-60

MAINTENANCE OF TRAFFIC GENERAL NOTES

WOO-96-16.21



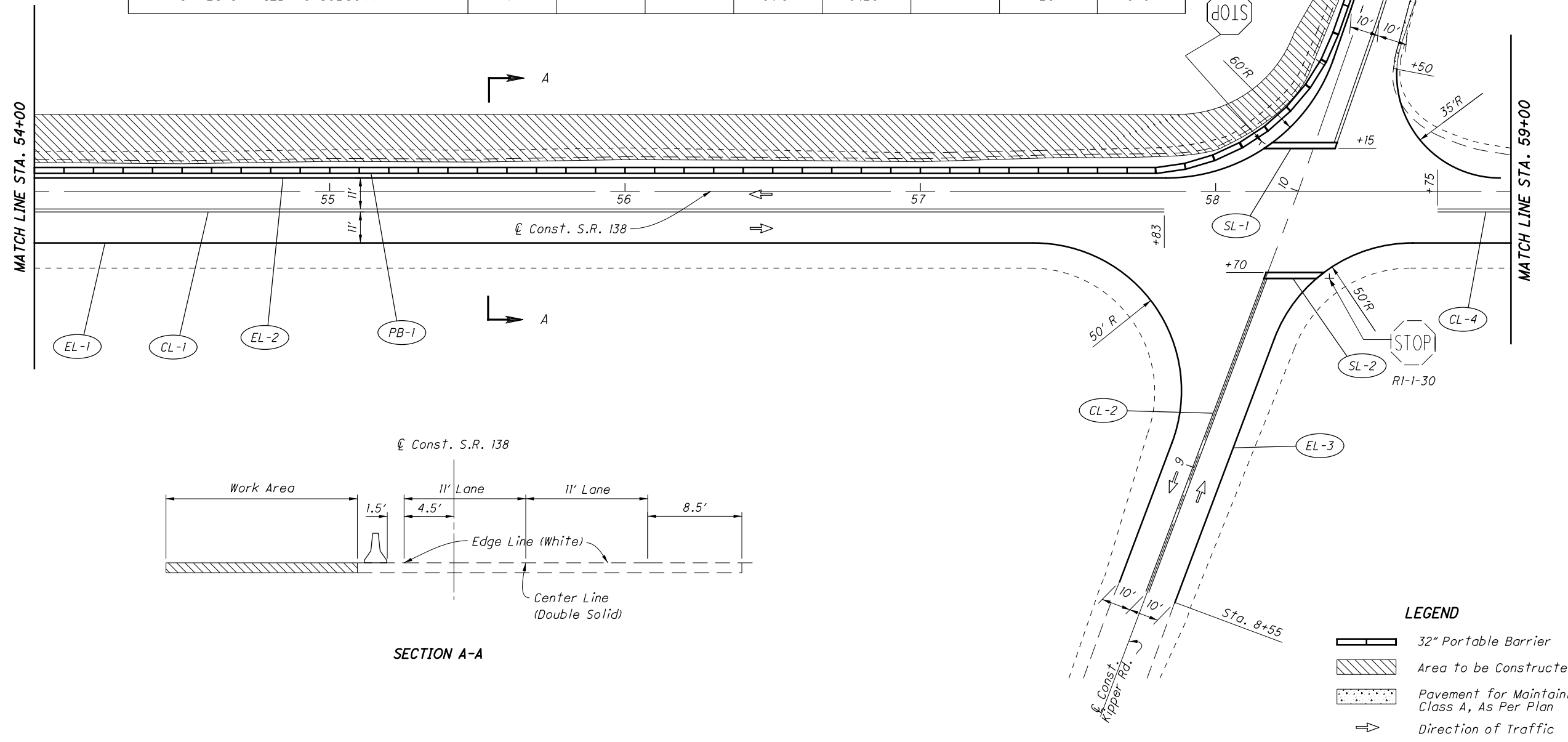
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HORIZONTAL SCALE IN FEET

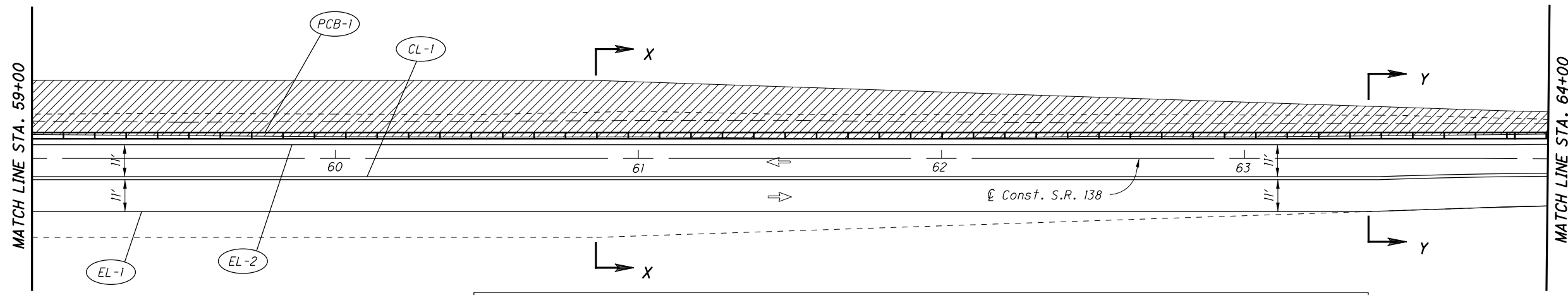
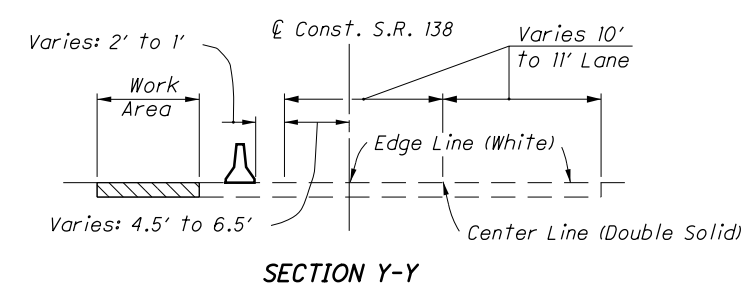
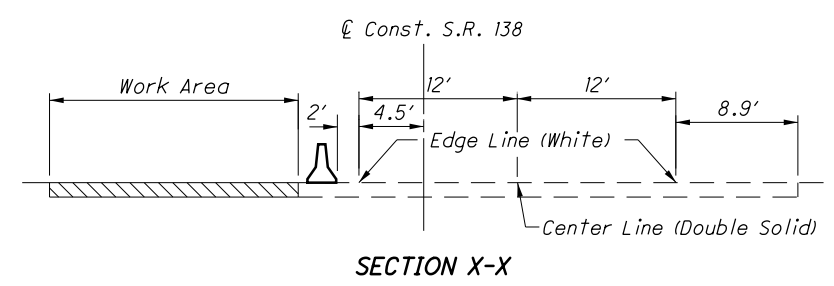
CALCULATED
CAN
CHECKED
DEK

MAINTENANCE OF TRAFFIC PHASE FOUR
STA. 54+00 TO STA. 59+00

SCI-138-11.44

| ESTIMATED QUANTITIES | | | | | | | | | | |
|------------------------------|--------------------------------------|------|-----------------------------|----------------------------|------------------------|---|--|--|--|-----------------------|
| REF No. | Station to Station | SIDE | 614 | | | 615 | | | 622 | |
| | | | WORK ZONE IMPACT ATTENUATOR | BARRIER REFLECTOR, TYPE B2 | OBJECT MARKER, TWO WAY | WORK ZONE CENTER LINE, CLASS I, 740.06, TYPE I (DOUBLE SOLID) | WORK ZONE EDGE LINE, CLASS I, 704.06, TYPE I (WHITE) | WORK ZONE STOP LINE, CLASS I, 740.06, TYPE I | PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN | PORTABLE BARRIER, 32" |
| | | | EACH | EACH | EACH | MILE | MILE | FT | SY | FT |
| CL-1 | 54+00 to 57+83 | Rt. | | | | 0.07 | | | | |
| CL-2 | 8+55 Kipper Rd. to 9+70 Kipper Rd. | Rt. | | | | 0.02 | | | | |
| CL-3 | 10+15 Kipper Rd. to 11+45 Kipper Rd. | Lt. | | | | 0.03 | | | | |
| CL-4 | 58+75 to 59+00 | Rt. | | | | 0.01 | | | | |
| EL-1 | 54+00 to 8+55 Kipper Rd | Rt. | | | | | 0.09 | | | |
| EL-2 | 54+00 to 12+00 Kipper Rd. | Lt. | | | | | 0.12 | | | |
| EL-3 | 8+55 Kipper Rd. to 59+00 | Rt. | | | | | 0.04 | | | |
| EL-4 | 11+45 Kipper Rd. to 59+00 | Lt. | | | | | 0.03 | | | |
| SL-1 | 10+15 Kipper Rd | Lt. | | | | | | 24 | | |
| SL-2 | 9+70 Kipper Rd. | Rt. | | | | | | 20 | | |
| PCB-1 | 54+00 to 11+48 Kipper Rd. | Lt. | | 11 | 11 | | | | 578 | |
| IA-1 | 11+48 Kipper Rd. to 11+73 Kipper Rd. | Lt. | 1 | | | | | | | |
| TP-1 | 10+50 Kipper Rd. to 11+70 Kipper Rd. | Rt. | | | | | | 20 | | |
| TOTALS CARRIED TO SUBSUMMARY | | | 1 | 11 | 11 | 0.13 | 0.28 | 44 | 20 | 578 |

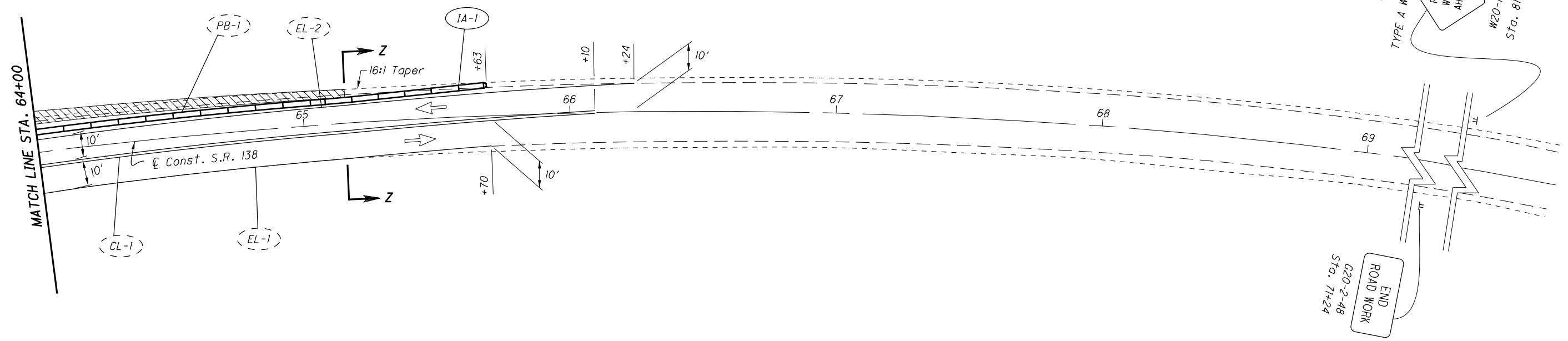
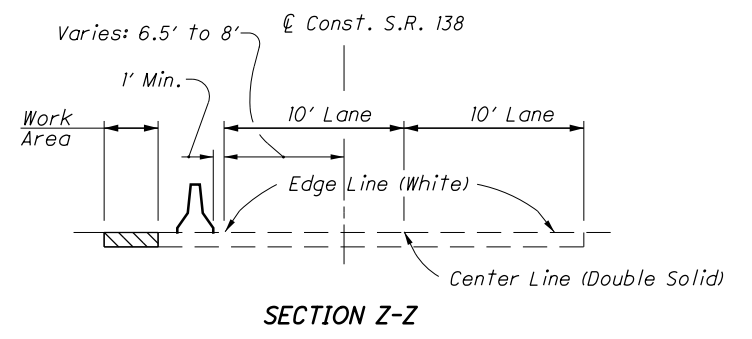




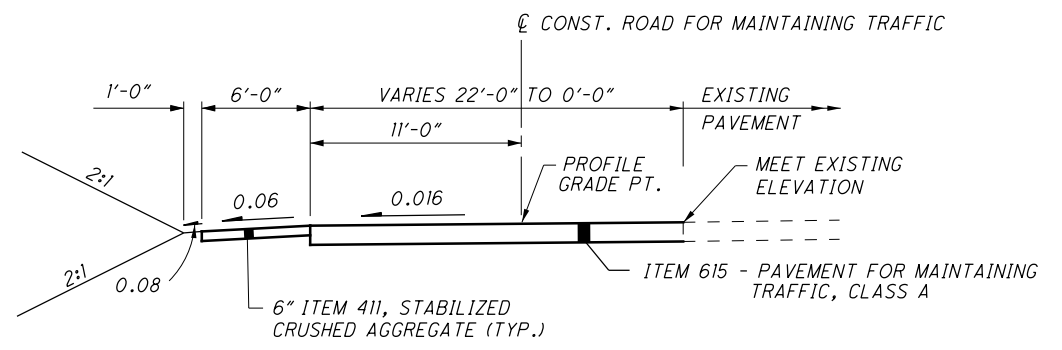
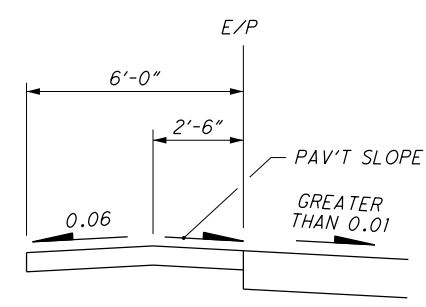
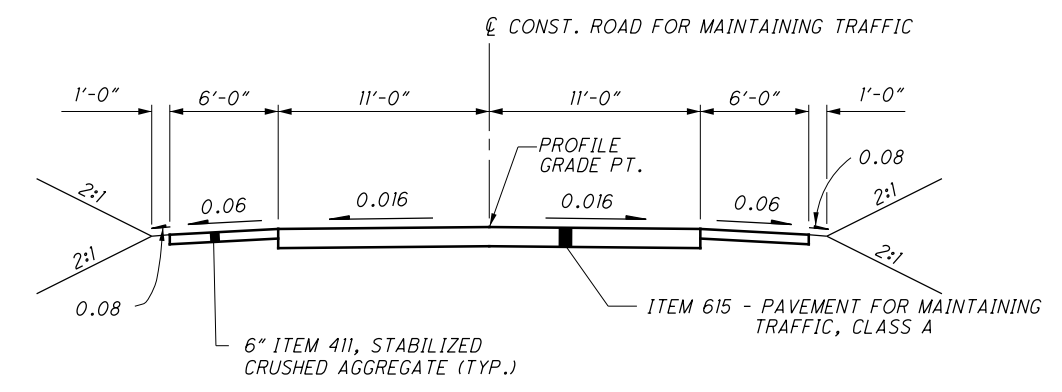
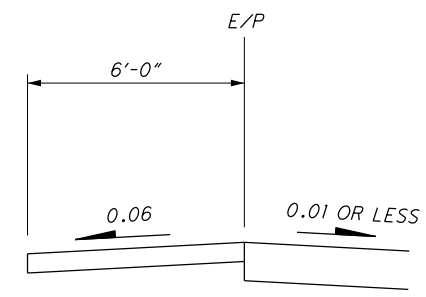
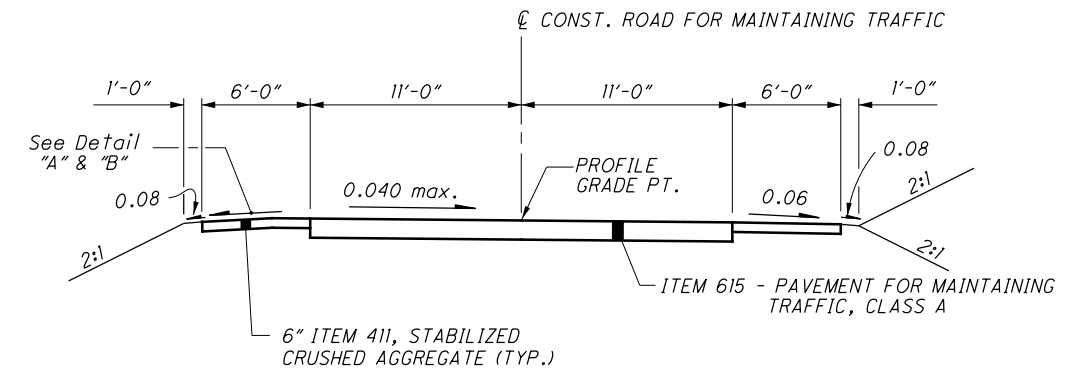
LEGEND

- 32" Portable Barrier
- Area to be Constructed
- Direction of Traffic

| REF No. | STATION TO STATION | SIDE | 614 | | | | 622 | |
|-------------------------------------|--------------------|------|--------------------------------------|----------------------------|------------------------|---|--|-----------------------|
| | | | REMOVE AND REPLACE IMPACT ATTENUATOR | BARRIER REFLECTOR, TYPE B2 | OBJECT MARKER, TWO WAY | WORK ZONE CENTER LINE, CLASS I, 740.06, TYPE 1 (DOUBLE SOLID) | WORK ZONE EDGE LINE, CLASS I, 704.06, TYPE 1 (WHITE) | PORTABLE BARRIER, 32" |
| | | | EACH | EACH | EACH | MILE | MILE | FT |
| CL-1 | 59+00 to 66+10 | Rt. | | | | 0.13 | | |
| EL-1 | 59+00 to 65+70 | Rt. | | | | | 0.13 | |
| EL-2 | 59+00 to 66+24 | Lt. | | | | | 0.14 | |
| PB-1 | 59+00 to 65+38 | Lt. | | 14 | 14 | | | 638 |
| IA-1 | 65+38 to 65+63 | Lt. | 1 | | | | | |
| TOTALS CARRIED TO SUBSUMMARY | | | 1 | 14 | 14 | 0.13 | 0.27 | 638 |



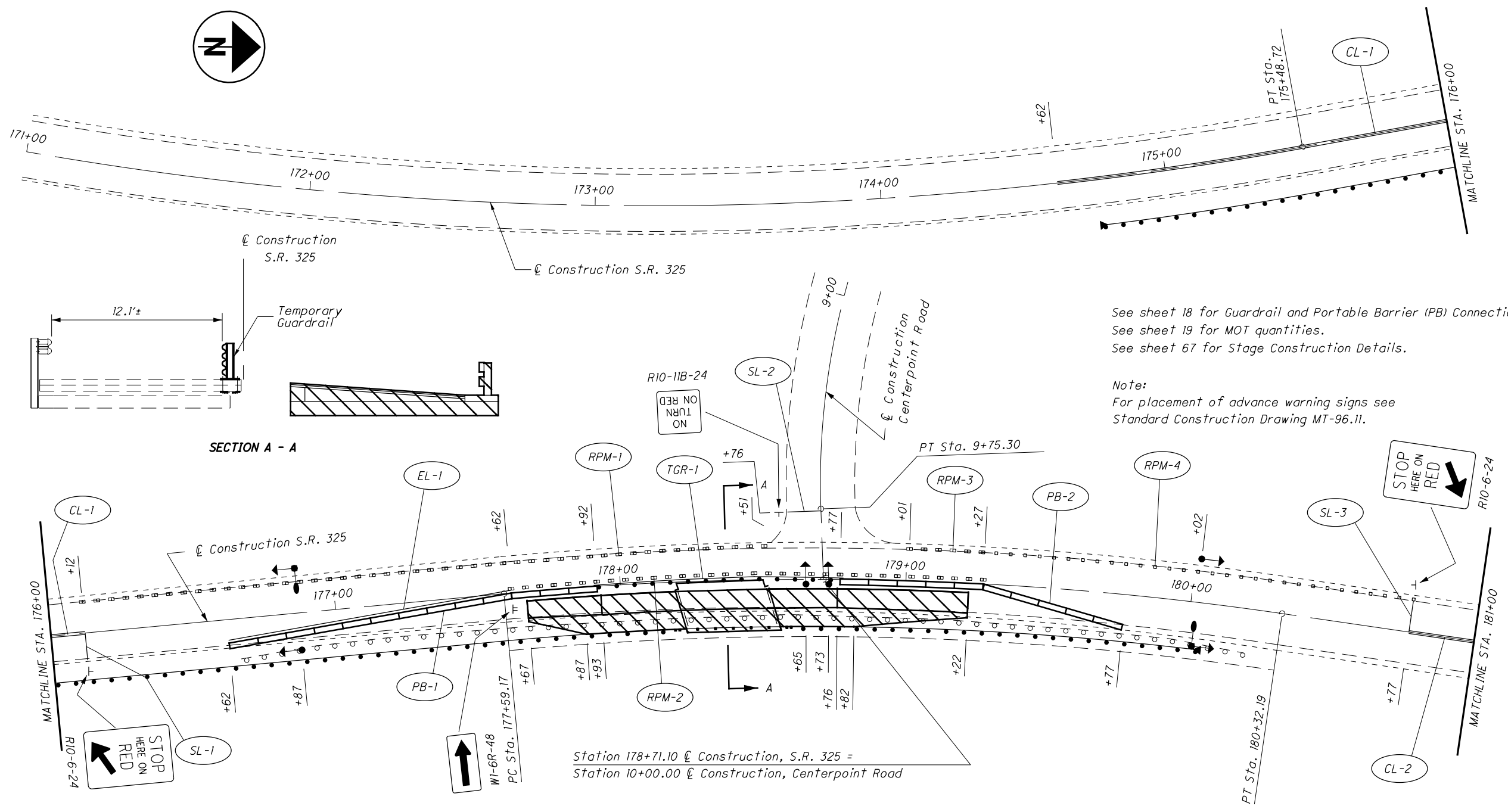
| ROAD FOR MAINTAINING TRAFFIC PAVEMENT EDGE ELEVATIONS | | |
|--|---------|--------------------|
| 11' LEFT | STATION | 11' RIGHT |
| MEET EXISTING | 5+65 | MEET EXISTING |
| 846.84 | 5+75 | 847.72 |
| 845.60 | 6+00 | 846.23 |
| 844.52 | 6+25 | 844.90 |
| 843.57 | 6+50 | 843.71 |
| 842.75 | 6+75 | 842.67 |
| 842.06 | 7+00 | 841.78 |
| 841.51 | 7+25 | 841.05 |
| 841.10 | 7+50 | 840.45 |
| 840.84 | 7+75 | 839.99 |
| 840.54 | 8+00 | 839.85 |
| 840.36 | 8+25 | 839.88 |
| 840.32 | 8+50 | 840.02 |
| 840.42 | 8+75 | 840.23 |
| 840.66 | 9+00 | 840.58 |
| 841.07 | 9+25 | 841.07 |
| 841.70 | 9+50 | 841.70 |
| 842.48 | 9+75 | 842.48 |
| 843.40 | 10+00 | 843.40 |
| 844.46 | 10+25 | 844.46 |
| 845.66 | 10+50 | 845.66 |
| 847.01 | 10+75 | 847.01 |
| 848.49 | 11+00 | 848.49 |
| 850.08 | 11+25 | 850.08 |
| 851.58 | 11+50 | 851.54 |
| 852.99 | 11+75 | 852.84 |
| 854.21 | 12+00 | 853.98 |
| 855.25 | 12+25 | 854.95 |
| 856.10 | 12+50 | 855.76 |
| 856.75 | 12+75 | 856.41 |
| 857.13 | 13+00 | 857.01 |
| 857.33 | 13+25 | 857.45 |
| MEET TAPER SECTION | 13+50 | MEET TAPER SECTION |



TEMPORARY ROAD TYPICAL SECTIONS AND ELEVATIONS

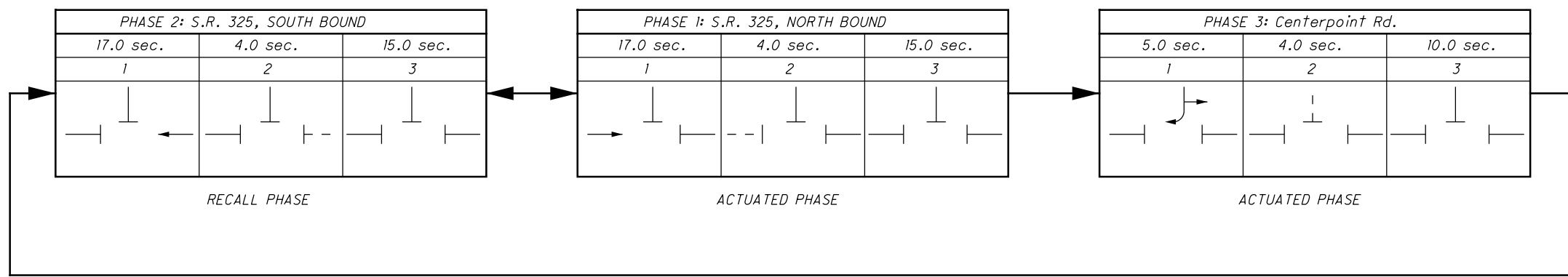
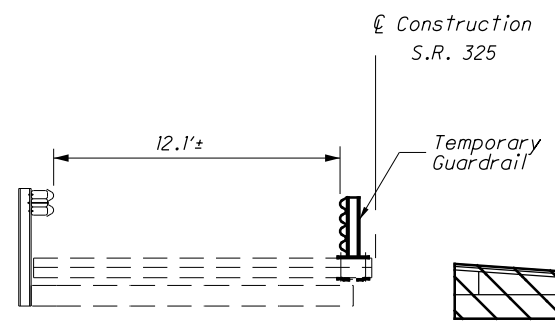
LIC-599-8.54

NOTE: FOR PLAN & PROFILE OF ROAD FOR MAINTAINING TRAFFIC, SEE SHEET 99.



See sheet 18 for Guardrail and Portable Barrier (PB) Connection Details.
See sheet 19 for MOT quantities.
See sheet 67 for Stage Construction Details.

Note:
For placement of advance warning signs see Standard Construction Drawing MT-96.11.



PHASE 1 - SIGNAL TIMING DIAGRAM

| REF NO. | STORM SEWER PROFILE SHEET NO. | STATION | | SIDE | 202 | | 601 | 602 | 611 | | | | | | | | | | 605 | 670 | SP 1307-2 | | |
|---|-------------------------------|---------|--------|-------|-----------------------------|---------------------|---|------------------|--------------------|---------------------|---------------------|---------------------|---------------------|-----------------------------|--------------------|---------------------|--------------------|---------------------|-----------------------------|--------------------------|----------------------|--|--|
| | | | | | PIPE REMOVED, 24" AND UNDER | CATCH BASIN REMOVED | ROCK CHANNEL PROTECTION, TYPE B WITH FILTER | CONCRETE MASONRY | 6" CONDUIT, TYPE F | 12" CONDUIT, TYPE B | 15" CONDUIT, TYPE C | 18" CONDUIT, TYPE B | 24" CONDUIT, TYPE B | 42" CONDUIT, TYPE B, 706.02 | CATCH BASIN, NO. 4 | CATCH BASIN, NO. 4A | CATCH BASIN, NO. 5 | CATCH BASIN, NO. 5A | 4" SHALLOW PIPE UNDERDRAINS | DITCH EROSION PROTECTION | DATE: JULY 2016 | | |
| | | | | | | | | | | | | | | | | | | | | | BENDS AND BRANCHES | | |
| | | | | | | | | | | | | | | | | | | | | | FOR INFORMATION ONLY | | |
| FT | EACH | CY | CY | FT | FT | FT | FT | FT | FT | FT | EACH | EACH | EACH | EACH | FT | SY | 6"X6" TEE | 6"X6" CROSS | 6"X90° BEND | | | | |
| FROM | TO | | | | | | | | | | | | | | | | EACH | EACH | EACH | | | | |
| D1 | 209 & 210 | 360+00 | 364+00 | ℄ | | | | | | 400 | | | | 1 | | | | | 125 | | | | |
| D2 | 210 | | 364+00 | RT | | | | | | 200 | | | | | | | | | 250 | | | | |
| D3 | 154 | | 363+60 | LT&RT | | | | | 160 | | | | | | | | | | | | | | |
| D4 | 154 | | 364+00 | LT | | | | | | | | | | | | | | | 125 | | | | |
| D5 | 154 | | 364+00 | ℄ | | | | | | | | | | | | | | | | | | | |
| D6 | 154 | | 364+00 | RT | | | 6 | | | | | | | | | | | | | | | | |
| D7 | 154 | | 364+00 | LT | | | | | | | | | | | | | | | | | | | |
| D8 | 210 | 364+00 | 365+00 | RT | | | | | | 100 | | | | | | | 1 | | 197 | | | | |
| D9 | 210 | 364+00 | 365+35 | ℄ | | | | | | | | | | | | | | | 226 | | | | |
| D10 | 210 | 364+00 | 365+75 | LT | | | | | | 175 | | | | | | | 1 | | 259 | | | | |
| D11 | 157 | | 368+20 | ℄ | | | | | | | | | 64 | | | | | | | | | | |
| D12 | 157 | | 368+20 | LT | | | | | | | | | 68 | | | | | | | | | | |
| D13 | 157 | | 368+20 | RT | | | 1 | 0.4 | | | | | | | | | | | | | | | |
| D14 | 212 | 368+20 | 371+00 | LT | | | | | | | | | | | | | | | 125 | | | | |
| D15 | 212 | 368+20 | 371+00 | ℄ | | | | | | 280 | | | | | | | | | 125 | | | | |
| D16 | 212 | 368+20 | 371+00 | RT | | | | | | 280 | | | | | | | | | 125 | | | | |
| R1 | | | 368+00 | RT | 20 | 1 | | | | | | | | | | | | | | | | | |
| U1 | | 359+90 | 363+97 | RT | | | | | | 20 | | | | | | | | | 407 | | 1 | | |
| U2 | | 359+90 | 363+97 | LT | | | | | | 20 | | | | | | | | | 407 | | 1 | | |
| U3 | | 359+90 | 363+97 | LT | | | | | | 22 | | | | | | | | | 814 | 1 | 1 | | |
| U4 | | 359+90 | 363+97 | RT | | | | | | 22 | | | | | | | | | 814 | 1 | 1 | | |
| U5 | | 364+03 | 368+18 | RT | | | | | | 20 | | | | | | | | | 415 | 1 | | | |
| U6 | | 364+03 | 368+18 | RT | | | | | | 22 | | | | | | | | | 830 | 1 | 1 | | |
| U7 | | 364+03 | 368+18 | LT | | | | | | 22 | | | | | | | | | 830 | 1 | 1 | | |
| U8 | | 364+03 | 368+18 | LT | | | | | | 20 | | | | | | | | | 415 | 1 | | | |
| U9 | | 368+22 | 371+00 | LT | | | | | | 22 | | | | | | | | | 556 | 1 | 1 | | |
| U10 | | 368+22 | 371+00 | RT | | | | | | 22 | | | | | | | | | 556 | 1 | 1 | | |
| U11 | | 368+22 | 371+00 | RT | | | | | | 20 | | | | | | | | | 278 | | 1 | | |
| U12 | | 368+22 | 371+00 | LT | | | | | | 20 | | | | | | | | | 278 | | 1 | | |
| U13 | | 371+00 | 373+50 | LT | | | | | | | | | | | | | | | 500 | 2 | | | |
| U14 | | 371+00 | 373+50 | LT | | | | | | | | | | | | | | | 250 | 1 | | | |
| U15 | | 371+00 | 373+50 | RT | | | | | | | | | | | | | | | 500 | 2 | | | |
| U16 | | 371+00 | 373+50 | RT | | | | | | | | | | | | | | | 250 | 1 | | | |
| ALL QUANTITIES FROM PLAN & PROFILE SHEET 81 | | | | | | | | | | | | | | | | | | | | | | | |
| TOTALS CARRIED TO GENERAL SUMMARY | | | | | 20 | 1 | 7 | 0.4 | 252 | 160 | 1850 | 68 | 141 | 144 | 4 | 1 | 7 | 2 | 8100 | 1557 | | | |

CALCULATED: MTG
 CHECKED: CJM
ESTIMATED QUANTITIES
LUC-76-31.48
 82
 488

SHEET NUMBER

OFFICE CALCS 11 82 83 84 85 86 87 88 89 91 105 157 255 259

ITEM

ITEM EXT.

GRAND TOTAL

UNIT

DESCRIPTION

SP 1307-3(a) DATE: JULY 2016

SEE SHEET NO.

CALCULATED JKP CHECKED FGW

ROADWAY

Table with columns: OFFICE CALCS, SHEET NUMBER, ITEM, ITEM EXT., GRAND TOTAL, UNIT, DESCRIPTION, SEE SHEET NO. Rows include items 201-204, 606, 607, 608, 609, 610, 623.

EROSION CONTROL

Table with columns: SHEET NUMBER, ITEM, ITEM EXT., GRAND TOTAL, UNIT, DESCRIPTION, SEE SHEET NO. Rows include items 601, 659.

DRAINAGE

Table with columns: SHEET NUMBER, ITEM, ITEM EXT., GRAND TOTAL, UNIT, DESCRIPTION, SEE SHEET NO. Rows include items 605, 611, 612, 613, 614, 615, 616, 617, 618, 619, 620.

GENERAL SUMMARY

TRU-99-13.48

79 267

SHEET NUMBER

OFFICE CALCS 196

FIG. 1307-3(b)
DATE: JULY 2016

SEE SHEET NO. CALCULATED JKP CHECKED FGW

| ITEM | ITEM EXT. | GRAND TOTAL | UNIT | DESCRIPTION | SEE SHEET NO. |
|-------------------|-----------|-------------|------|---|--|
| PAVEMENT | | | | | |
| 312 | 01000 | 312 | SY | PARTIAL DEPTH PAVEMENT REPAIR | |
| 4140 | 01000 | 4140 | SY | PAVEMENT REPAIR | |
| 9005 | 10011 | 9005 | SY | FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS OC1, AS PER PLAN | 16 |
| 2894 | 10161 | 2894 | SY | FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS OC MS, AS PER PLAN | 16 |
| 34828 | 20000 | 34828 | FT | FULL DEPTH PAVEMENT SAWING | |
| 17759 | 20000 | 17759 | CY | AGGREGATE BASE | |
| 3892 | 13000 | 3892 | SY | 9" CONCRETE BASE | |
| 127 | 10000 | 127 | GAL | TACK COAT | |
| 5813 | 10000 | 5813 | GAL | PRIME COAT | |
| 1029 | 14001 | 1029 | SY | 9" REINFORCED CONCRETE PAVEMENT, AS PER PLAN | 12 |
| 31690 | 15001 | 31690 | SY | 10" REINFORCED CONCRETE PAVEMENT, AS PER PLAN | 12 |
| 6783 | 13001 | 6783 | SY | 9" NON-REINFORCED CONCRETE PAVEMENT, AS PER PLAN | 12 |
| 50342 | 17001 | 50342 | SY | VARIABLE THICKNESS NON-REINFORCED CONCRETE PAVEMENT, AS PER PLAN | 12 |
| WATER WORK | | | | | |
| 3649 | 638 | 02504 | 3649 | FT | 12" WATER MAIN DUCTILE IRON PIPE ANSI CLASS 53, MECHANICAL JOINTS AND FITTINGS |
| 2481 | 638 | 02604 | 2481 | FT | 12" WATER MAIN DUCTILE IRON PIPE ANSI CLASS 53, BOLTLESS-RESTRAINED, JOINTS AND FITTINGS |
| 2107 | 638 | 02700 | 2107 | FT | 12" WATER MAIN DUCTILE IRON PIPE ANSI CLASS 55, BALL AND SOCKET JOINTS AND FITTINGS |
| 1608 | 638 | 02800 | 1608 | FT | 12" WATER MAIN POLYVINYL CHLORIDE PIPE AND FITTINGS, ASTM SDR 26 |
| 1142 | 638 | 02900 | 1142 | FT | 12" WATER MAIN POLYVINYL CHLORIDE PIPE AND FITTINGS, AWWA CLASS 150 |
| 438 | 638 | 04800 | 438 | FT | 3/4" COPPER SERVICE BRANCH |
| 464 | 638 | 05300 | 464 | FT | 3/4" POLYETHYLENE SERVICE BRANCH |
| 212 | 638 | 06704 | 212 | FT | 20" STEEL PIPE ENCASEMENT, OPEN CUT |
| 310 | 638 | 07310 | 310 | FT | 24" STEEL PIPE ENCASEMENT, BORED OR JACKED |
| 18 | 638 | 08100 | 18 | EACH | 12" GATE VALVE AND VALVE BOX |
| 16 | 638 | 09200 | 16 | EACH | 12" CUTTING-IN SLEEVE, VALVE AND VALVE BOX |
| 12 | 638 | 09700 | 12 | EACH | 12" X 6" TAPPING SLEEVE, VALVE AND VALVE BOX |
| 36 | 638 | 10200 | 36 | EACH | 6" FIRE HYDRANT |
| 10 | 638 | 10300 | 10 | EACH | FIRE HYDRANT EXTENDED AND ADJUSTED TO GRADE |
| 8 | 638 | 10500 | 8 | EACH | FIRE HYDRANT REMOVED AND RESET |
| 8 | 638 | 10600 | 8 | EACH | FIRE HYDRANT AND GATE VALVE REMOVED AND RESET |
| 12 | 638 | 10800 | 12 | EACH | VALVE BOX ADJUSTED TO GRADE |
| 6 | 638 | 10900 | 6 | EACH | SERVICE BOX ADJUSTED TO GRADE |
| 4 | 638 | 11100 | 4 | EACH | METER AND CHAMBER REMOVED AND RESET |

GENERAL SUMMARY

TRU-99-13.48

| SHEET NUMBER | | | | | | | | | | | PARTICIPATION | | ITEM | ITEM EXT. | GRAND TOTAL | UNIT | DESCRIPTION | FIG. 1307-3(c) DATE: JULY 2016 | | SEE SHEET NO. | CALCULATED | JKP CHECKED | FGW | |
|--|-------|----|----|-----|-----|--|--|------|--|--|---------------|---------|----------|-----------|-------------|---|-------------|-----------------------------------|--|---------------|------------|-------------|-----|-----|
| 16 | 18 | 23 | 55 | 192 | 262 | | | | | | | | | | | | | | | | | | | |
| RETAINING WALLS | | | | | | | | | | | | | | | | | | | | | | | | |
| OPTION A: REINFORCED EARTH WALL | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | 1710 | | | | 203 | 20000 | 1710 | CY | EMBANKMENT | | | | | | | | |
| | | | | | | | | 3474 | | | | 203 | 35000 | 3474 | CY | GRANULAR EMBANKMENT | | | | | | | | |
| | | | | | | | | LS | | | | 503 | 11100 | LS | | COFFERDAMS AND EXCAVATION BRACING | | | | | | | | |
| | | | | | | | | 1124 | | | | 503 | 21101 | 1124 | CY | UNCLASSIFIED EXCAVATION, AS PER PLAN | | | | | | | | 108 |
| | | | | | | | | 4766 | | | | SPECIAL | 61050010 | 4766 | SF | RETAINING WALL, MISC.: REINFORCED EARTH WALL SYSTEM | | | | | | | | 190 |
| OPTION B: RETAINED EARTH WALL | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | 1636 | | | | 203 | 20000 | 1636 | CY | EMBANKMENT | | | | | | | | |
| | | | | | | | | 3584 | | | | 203 | 35000 | 3584 | CY | GRANULAR EMBANKMENT | | | | | | | | |
| | | | | | | | | LS | | | | 503 | 11100 | LS | | COFFERDAMS AND EXCAVATION BRACING | | | | | | | | |
| | | | | | | | | 1150 | | | | 503 | 21101 | 1150 | CY | UNCLASSIFIED EXCAVATION, AS PER PLAN | | | | | | | | 108 |
| | | | | | | | | 4738 | | | | SPECIAL | 61050010 | 4738 | SF | RETAINING WALL, MISC.: RETAINED EARTH WALL SYSTEM | | | | | | | | 190 |
| BUILDING DEMOLITION | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | LS | | | | 202 | 56000 | LS | | BUILDING DEMOLISHED: PARCEL NO. 11-WD-1, 1 STORY BRICK BUILDING | | | | | | | | |
| | | | | | | | | LS | | | | 202 | 56000 | LS | | BUILDING DEMOLISHED: PARCEL NO. 13-T, 1 STORY BLOCK BUILDING | | | | | | | | |
| | | | | | | | | LS | | | | 202 | 56000 | LS | | BUILDING DEMOLISHED: PARCEL NO. 13-WL, 2 STORY BRICK BUILDING | | | | | | | | |
| | | | | | | | | LS | | | | 202 | 56000 | LS | | BUILDING DEMOLISHED: PARCEL NO. 19-T, 1 STORY METAL BUILDING | | | | | | | | |
| STRUCTURES OVER 20 FOOT SPAN | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | | | STRUCTURE TRU-99-1924 GENERAL SUMMARY | | | | | | | | 229 |
| | | | | | | | | | | | | | | | | STRUCTURE TRU-99-2056 GENERAL SUMMARY | | | | | | | | 236 |
| | | | | | | | | 878 | | | | SPECIAL | 51272000 | 878 | SY | EPOXY WATERPROOFING OVERLAY (1/4" THICK) | | | | | | | | 190 |
| MAINTENANCE OF TRAFFIC | | | | | | | | | | | | | | | | | | | | | | | | |
| | 10 | | | | | | | | | | | 614 | 11110 | 10 | HOURLY | LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE | | | | | | | | |
| | 5 | | | | | | | | | | | 614 | 12460 | 5 | EACH | WORK ZONE MARKING SIGN | | | | | | | | |
| | | | 38 | | | | | | | | | 614 | 12470 | 38 | EACH | WORK ZONE SPEED LIMIT SIGN | | | | | | | | |
| | 100 | | | | | | | | | | | 614 | 12500 | 100 | EACH | REPLACEMENT SIGN | | | | | | | | |
| | 200 | | | | | | | | | | | 614 | 12600 | 200 | EACH | REPLACEMENT DRUM | | | | | | | | |
| | | | | | | | | | | | | 614 | 12800 | 1528 | EACH | WORK ZONE RAISED PAVEMENT MARKER | | | | | | | | |
| | | | | | | | | | | | | 614 | 13100 | 1201 | EACH | BARRIER REFLECTOR | | | | | | | | |
| | 14.00 | | | | | | | | | | | 614 | 20300 | 14.00 | MILE | WORK ZONE LANE LINE, CLASS I, 740.06, TYPE II | | | | | | | | |
| | | | | | | | | | | | | 614 | 21300 | 0.11 | MILE | WORK ZONE CENTER LINE, CLASS I, 740.06, TYPE II | | | | | | | | |
| | | | | | | | | | | | | 614 | 21700 | 0.26 | MILE | WORK ZONE CENTER LINE, CLASS II, 740.06, TYPE II | | | | | | | | |
| | | | | | | | | | | | | 614 | 22000 | 4.33 | MILE | WORK ZONE EDGE LINE, CLASS I | | | | | | | | |
| | | | | | | | | | | | | 614 | 22300 | 7.34 | MILE | WORK ZONE EDGE LINE, CLASS I, 740.06, TYPE II | | | | | | | | |
| | | | | | | | | | | | | 614 | 26600 | 48 | FT | WORK ZONE STOP LINE, CLASS I, 740.06, TYPE II | | | | | | | | |
| | | | | | | | | | | | | 614 | 28600 | 5692 | FT | WORK ZONE GORE MARKING, CLASS II, 740.06, TYPE II | | | | | | | | |
| | | | | | | | | | | | | 615 | 10000 | LS | | ROADS FOR MAINTAINING TRAFFIC | | | | | | | | |
| | | | | | | | | | | | | 615 | 20001 | 944 | SY | PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN | | | | | | | | 17 |
| | 30 | | | | | | | | | | | 616 | 10000 | 30 | M GAL | WATER | | | | | | | | |
| | 10 | | | | | | | | | | | 616 | 20000 | 10 | TON | CALCIUM CHLORIDE | | | | | | | | |
| | | | | | | | | | | | | 622 | 41001 | 30280 | FT | PORTABLE BARRIER, 32", AS PER PLAN | | | | | | | | 58 |
| INCIDENTALS | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | 614 | 11000 | LS | | MAINTAINING TRAFFIC | | | | | | | | |
| | | | | | | | | | | | | 619 | 16020 | 8 | MNTH | FIELD OFFICE, TYPE C | | | | | | | | |
| | | | | | | | | | | | | 623 | 10000 | LS | | CONSTRUCTION LAYOUT STAKES AND SURVEYING | | | | | | | | |
| | | | | | | | | | | | | 624 | 10000 | LS | | MOBILIZATION | | | | | | | | |

GENERAL SUMMARY

TRU-99-13.48

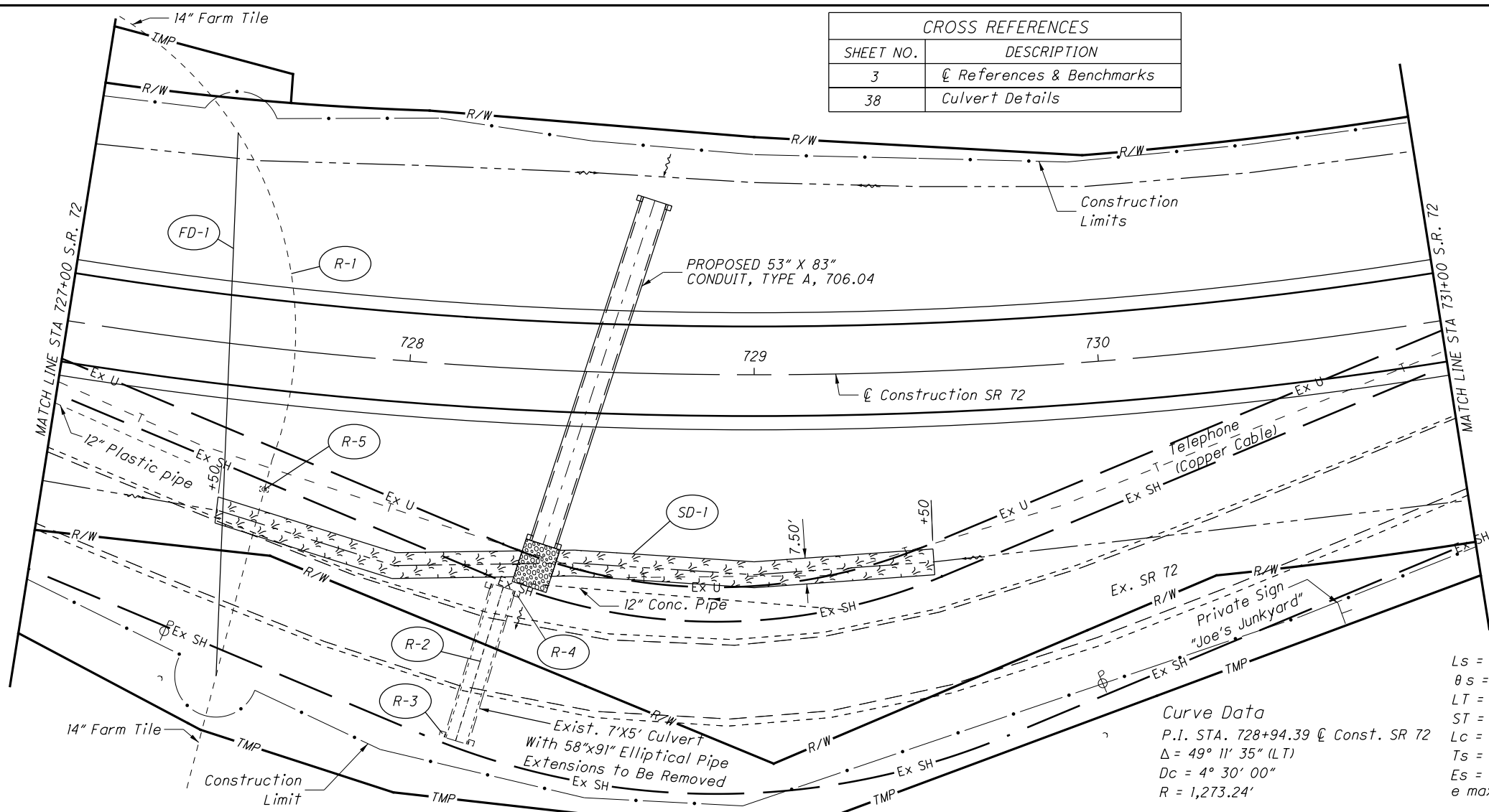
| SHEET NUMBER | | | | PARTICIPATION | | ITEM | ITEM EXT. | GRAND TOTAL | UNIT | DESCRIPTION | SP 1307-5 DATE: JULY 2016 | SEE SHEET NO. | CALCULATED JKP CHECKED FGW |
|--------------|----|--|--|-----------------------|-----------|------|-----------|-------------|------|-----------------|---|---------------|-------------------------------------|
| 87 | 96 | | | CITY, STATE & FEDERAL | 100% CITY | | | | | | | | |
| | | | | | | | | | | TRAFFIC CONTROL | | | |
| | | | | | 161 | | 621 | 00100 | 161 | EACH | RPM | | |
| | | | | | | 146 | 630 | 02100 | 146 | FT | GROUND MOUNTED SUPPORT, NO. 2 POST | | |
| | | | | | | 229 | 630 | 03100 | 229 | FT | GROUND MOUNTED SUPPORT, NO. 3 POST | | |
| | | | | | | 4 | 630 | 79500 | 4 | EACH | SIGN SUPPORT ASSEMBLY, POLE MOUNTED | | |
| | | | | | | 96 | 630 | 80100 | 96 | SF | SIGN, FLAT SHEET | | |
| | | | | | | 11 | 630 | 85000 | 11 | EACH | REMOVAL OF GROUND MOUNTED SIGN AND STORAGE | | |
| | | | | | | 14 | 630 | 86002 | 14 | EACH | REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL | | |
| | | | | | 2.49 | 0.80 | 644 | 00100 | 3.29 | MILE | EDGE LINE | | |
| | | | | | 1.06 | 0.30 | 644 | 00200 | 1.36 | MILE | LANE LINE | | |
| | | | | | 0.98 | 0.27 | 644 | 00300 | 1.25 | MILE | CENTER LINE | | |
| | | | | | 430 | 938 | 644 | 00400 | 1368 | FT | CHANNELIZING LINE | | |
| | | | | | | 146 | 644 | 00500 | 146 | FT | STOP LINE | | |
| | | | | | | 313 | 644 | 00600 | 313 | FT | CROSSWALK LINE | | |
| | | | | | 180 | 270 | 644 | 00700 | 450 | FT | TRANSVERSE/DIAGONAL LINE | | |
| | | | | | | 24 | 644 | 00900 | 24 | SF | ISLAND MARKING | | |
| | | | | | 2 | 7 | 644 | 01300 | 9 | EACH | LANE ARROW | | |
| | | | | | | 8 | 644 | 01410 | 8 | EACH | WORD ON PAVEMENT, 96" | | |
| | | | | | | | | | | | TRAFFIC SIGNALS | | |
| | | | | | 122 | | 625 | 25400 | 122 | FT | CONDUIT, 2", 725.04 | | |
| | | | | | 180 | | 625 | 25500 | 180 | FT | CONDUIT, 3", 725.04 | | |
| | | | | | 182 | | 625 | 29000 | 182 | FT | TRENCH | | |
| | | | | | 120 | | 625 | 29600 | 120 | FT | TRENCH IN PAVED AREA, TYPE B | | |
| | | | | | 2 | | 625 | 30706 | 2 | EACH | PULL BOX, 725.08, 24" | | |
| | | | | | 7 | | 625 | 32000 | 7 | EACH | GROUND ROD | | |
| | | | | | 4 | | 632 | 00300 | 4 | EACH | VEHICULAR SIGNAL HEAD, 3 SECTION, 12" LENS, 1-WAY | | |
| | | | | | 2 | | 632 | 00500 | 2 | EACH | VEHICULAR SIGNAL HEAD, 5 SECTION, 12" LENS, 1-WAY | | |
| | | | | | 1 | | 632 | 01100 | 1 | EACH | VEHICULAR SIGNAL HEAD, 3 SECTION, 12" LENS, 2-WAY | | |
| | | | | | 8 | | 632 | 25000 | 8 | EACH | COVERING OF VEHICULAR SIGNAL HEAD | | |
| | | | | | 2 | | 632 | 27004 | 2 | EACH | LOOP DETECTOR UNIT | | |
| | | | | | 3 | | 632 | 27008 | 3 | EACH | LOOP DETECTOR UNIT, DELAY AND EXTENSION TYPE | | |
| | | | | | 139 | | 632 | 30200 | 139 | FT | MESSENGER WIRE, 7 STRAND, 3/8" DIAMETER WITH ACCESSORIES | | |
| | | | | | 823 | | 632 | 40300 | 823 | FT | SIGNAL CABLE, 3 CONDUCTOR, NO. 14 AWG | | |
| | | | | | 1168 | | 632 | 40500 | 1168 | FT | SIGNAL CABLE, 5 CONDUCTOR, NO. 14 AWG | | |
| | | | | | 100 | | 632 | 62700 | 100 | FT | INTERCONNECT CABLE, INTEGRAL MESSENGER WIRE TYPE, 7 CONDUCTOR, NO. 12 AWG | | |
| | | | | | 1601 | | 632 | 65200 | 1601 | FT | LOOP DETECTOR LEAD-IN CABLE | | |
| | | | | | 25 | | 632 | 67200 | 25 | FT | POWER CABLE, 2 CONDUCTOR, NO. 8 AWG | | |
| | | | | | 1 | | 632 | 70001 | 1 | EACH | POWER SERVICE, AS PER PLAN | 95 | |
| | | | | | 2 | | 632 | 85000 | 2 | EACH | COMBINATION STRAIN POLE, TYPE TC-81.10, DESIGN 10 | | |
| | | | | | 5 | | 632 | 89900 | 5 | EACH | PEDESTAL, 8', TRANSFORMER BASE | | |
| | | | | | 1 | | 632 | 90100 | 1 | EACH | REMOVAL OF TRAFFIC SIGNAL INSTALLATION | | |
| | | | | | 1 | | 633 | 01601 | 1 | EACH | CONTROLLER UNIT, TYPE 170E, WITH CABINET, TYPE 332, AS PER PLAN "A" | 95 | |
| | | | | | 1 | | 633 | 67100 | 1 | EACH | CABINET FOUNDATION | | |
| | | | | | 1 | | 633 | 67200 | 1 | EACH | CONTROLLER WORK PAD | | |

GENERAL SUMMARY

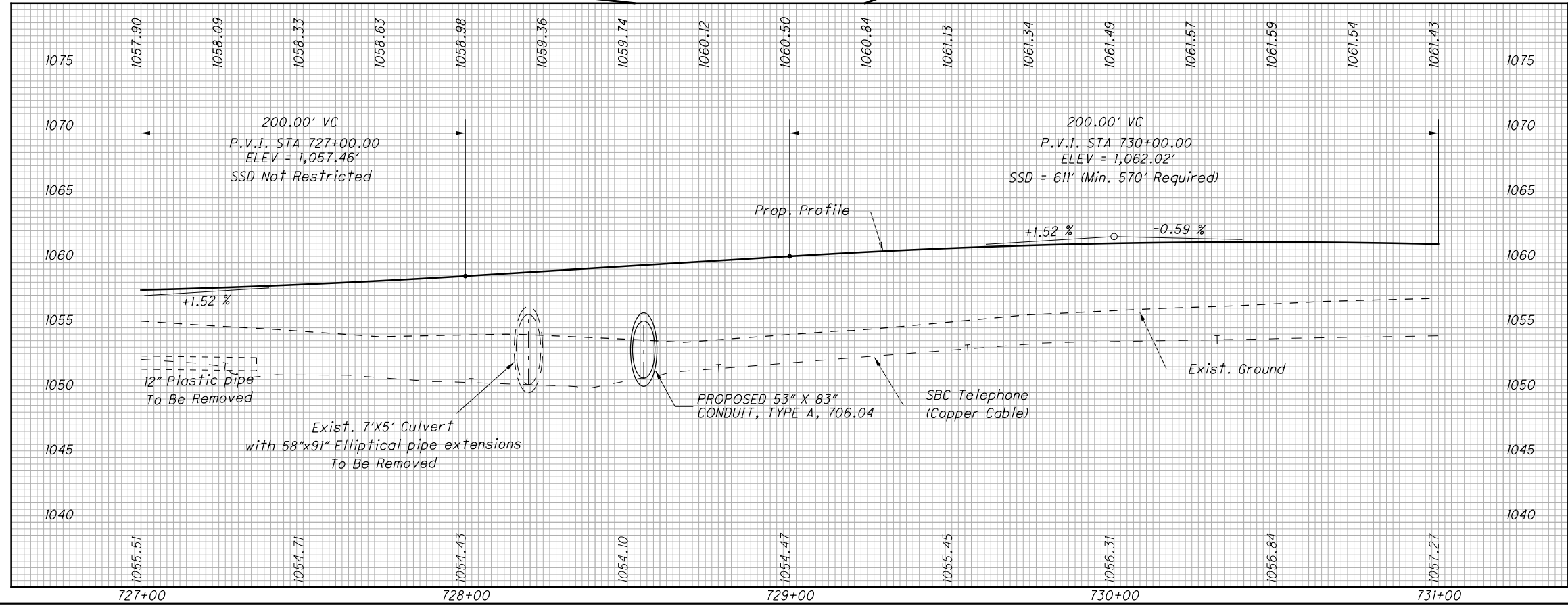
ATB-208-13.43



| CROSS REFERENCES | |
|------------------|---------------------------|
| SHEET NO. | DESCRIPTION |
| 3 | ☉ References & Benchmarks |
| 38 | Culvert Details |



Curve Data
 P.I. STA. 728+94.39 @ Const. SR 72
 $\Delta = 49^\circ 11' 35''$ (LT)
 $D_c = 4^\circ 30' 00''$
 $R = 1,273.24'$
 $L_s = 222.00'$
 $\theta_s = 4^\circ 59' 42''$
 $LT = 148.06'$
 $ST = 74.05'$
 $L_c = 871.18'$
 $T_s = 694.55'$
 $E_s = 128.83'$
 $e_{max} = 0.08$

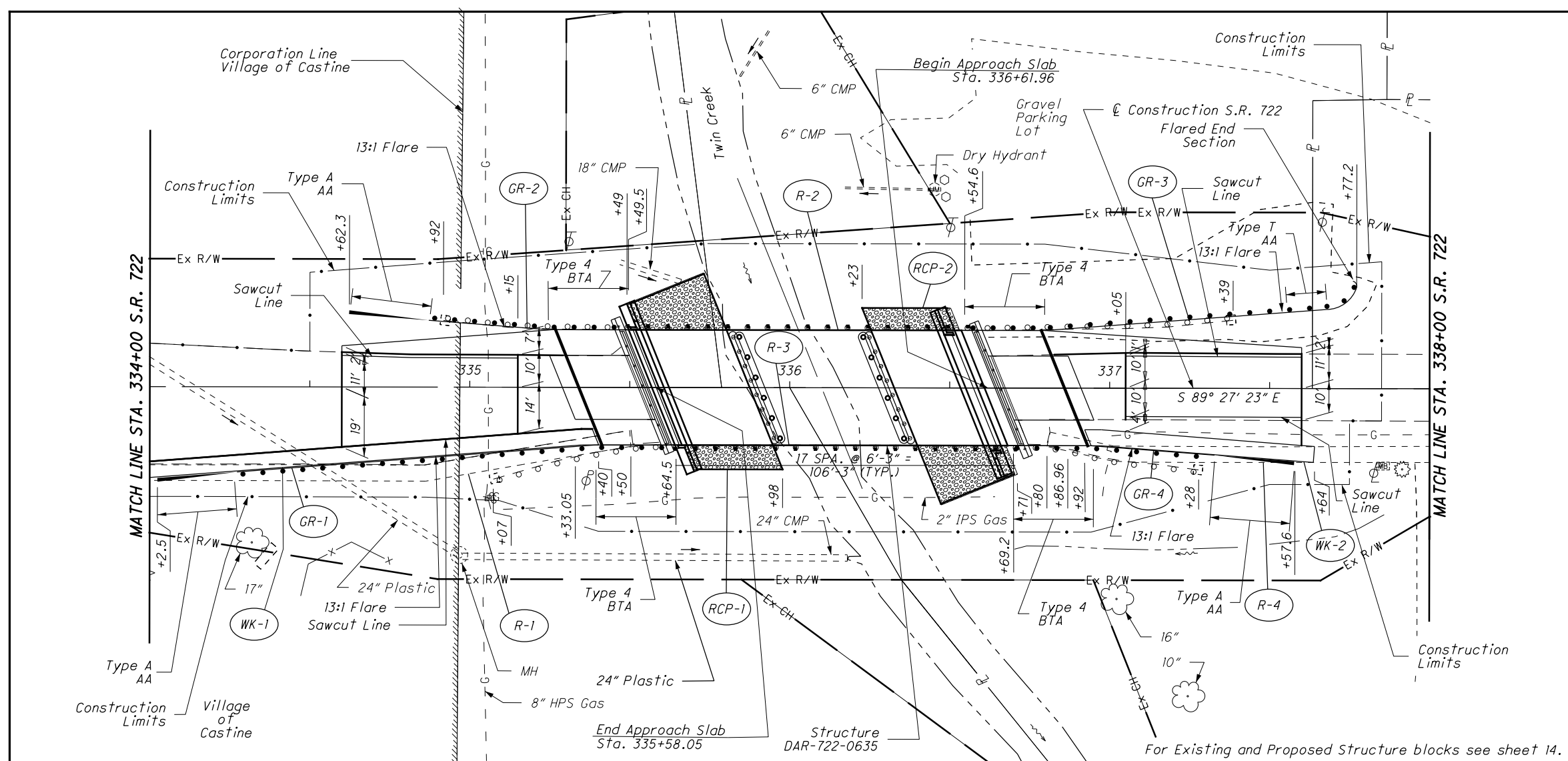


| REF NO. | STATION | | SIDE | DESCRIPTION | QTY | UNIT | AMOUNT |
|-----------------------------------|---------|--------|---------|---|-----|------|--------|
| | FROM | TO | | | | | |
| R-1 | 727+43 | 727+56 | RT & LT | EROSION CONTROL MAT, TYPE A | 167 | SY | 167 |
| R-2 | 728+15 | 728+22 | RT | 15\"/> | | | |
| R-3 | 728+17 | 728+23 | RT | REMOVAL MISC.: TILE JUNCTION BOX | 1 | EACH | 1 |
| R-4 | 728+27 | 728+35 | RT | REMOVAL MISC.: CONCRETE BOX CULVERT WITH 58\"/> | | | |
| R-5 | 728+51 | 728+53 | RT | PIPE REMOVED, 24\"/> | | | |
| FD-1 | 727+43 | 727+56 | RT & LT | HEADWALL REMOVED | 2 | EACH | 2 |
| SD-1 | 727+50 | 729+50 | RT | PIPE REMOVED, 24\"/> | | | |
| TOTALS CARRIED TO GENERAL SUMMARY | | | | | | | |

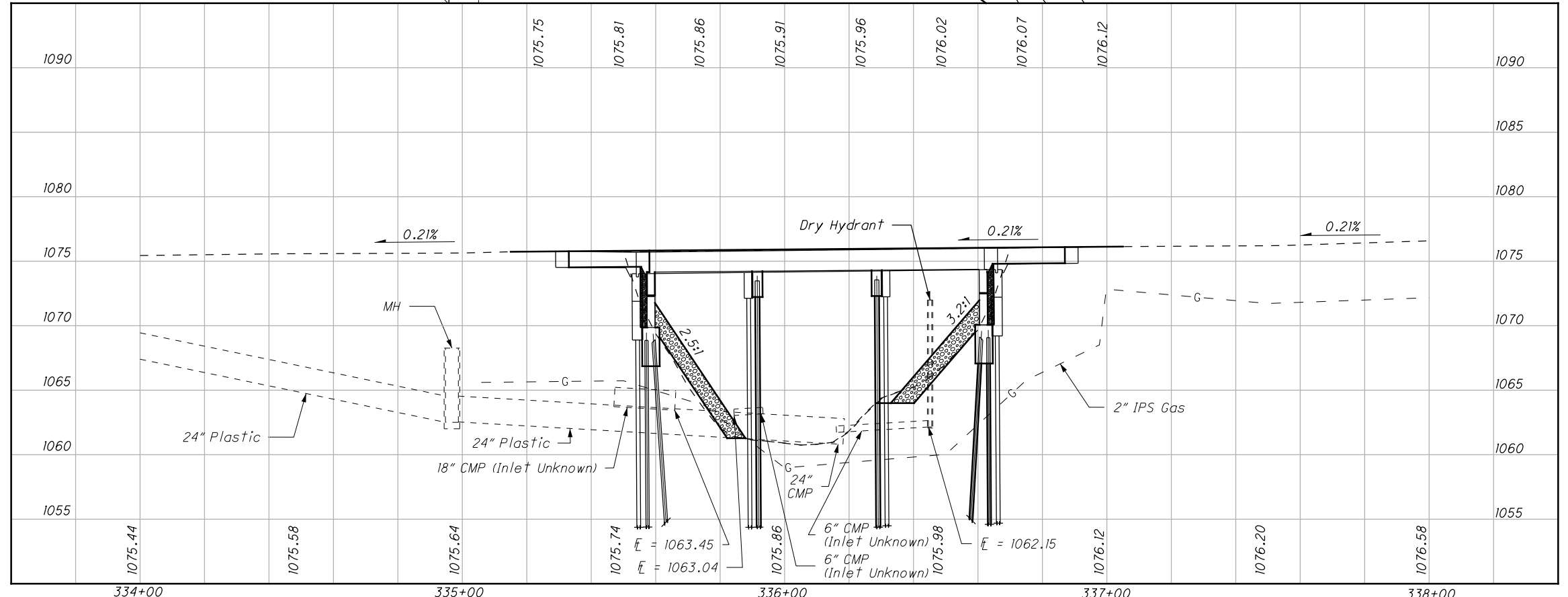
SCALE IN FEET
 HORIZONTAL
 0 10 20 40
 CALCULATED: MSO
 CHECKED: JAD

PLAN AND PROFILE
 STA 727+00 TO 731+00

GRE-72-13.67



For Existing and Proposed Structure blocks see sheet 14.



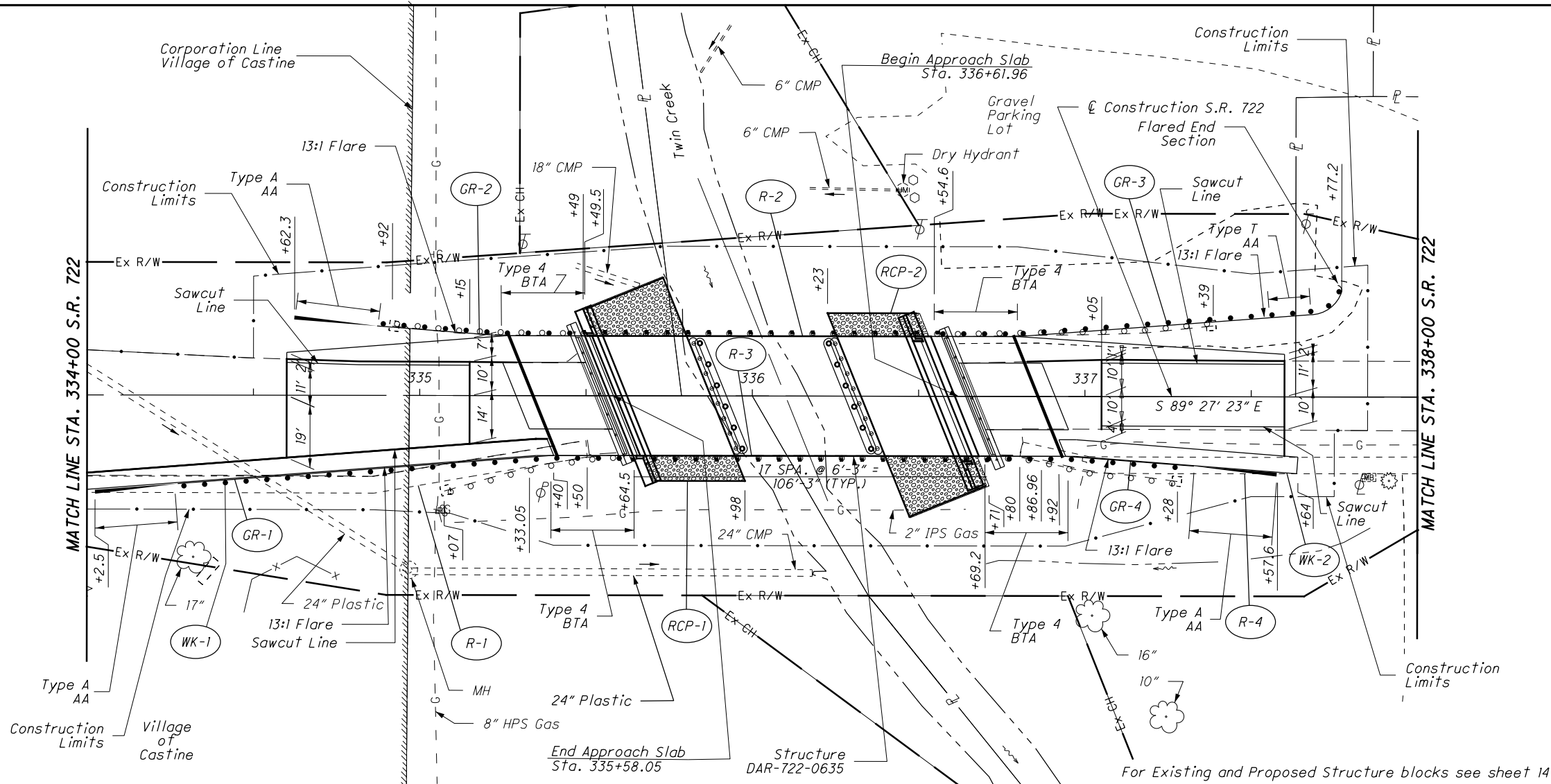
| REF NO. | STATION | | SIDE | QUANTITY | UNIT | TOTALS CARRIED TO GENERAL SUMMARY |
|--|----------|----------|--|----------|------|-----------------------------------|
| | FROM | TO | | | | |
| GR-1 | 334+02.5 | 335+49.5 | RT. | 1 | | |
| GR-2 | 334+62.3 | 335+64.5 | LT. | 1 | | |
| GR-3 | 336+54.6 | 337+77.2 | LT. | 1 | | |
| GR-4 | 336+69.2 | 337+67.6 | RT. | 1 | | |
| R-2 | 334+92 | 337+39 | LT. | 1 | | |
| R-3 | 335+07 | 337+28 | RT. | 1 | | |
| R-4 | 336+80 | 337+64 | RT. | 1 | | |
| RPC-1 | 335+49 | 335+98 | LT/RT | 1 | | |
| RPC-2 | 336+23 | 336+71 | LT/RT | 1 | | |
| WK-2 | 336+92 | 337+64 | RT. | 1 | | |
| TOTALS CARRIED TO GENERAL SUMMARY | | | | | | |
| 601 | | | ROCK CHANNEL PROTECTION TYPE C WITH FILTER | | CY | 250 |
| 608 | | | 4" CONCRETE WALK | | SF | 354 |
| 606 | | | BRIDGE TERMINAL ASSEMBLY, TYPE 4 | | EACH | 4 |
| 606 | | | ANCHOR ASSEMBLY, TYPE T | | EACH | 1 |
| 606 | | | ANCHOR ASSEMBLY, TYPE A | | EACH | 3 |
| 606 | | | GUARDRAIL TYPE 5 | | FT | 387.5 |
| 202 | | | GUARDRAIL REMOVED | | FT | 475 |
| 202 | | | WALK REMOVED | | SF | 405 |

PLAN AND PROFILE - S.R. 722
STA. 334+00 TO STA. 338+00

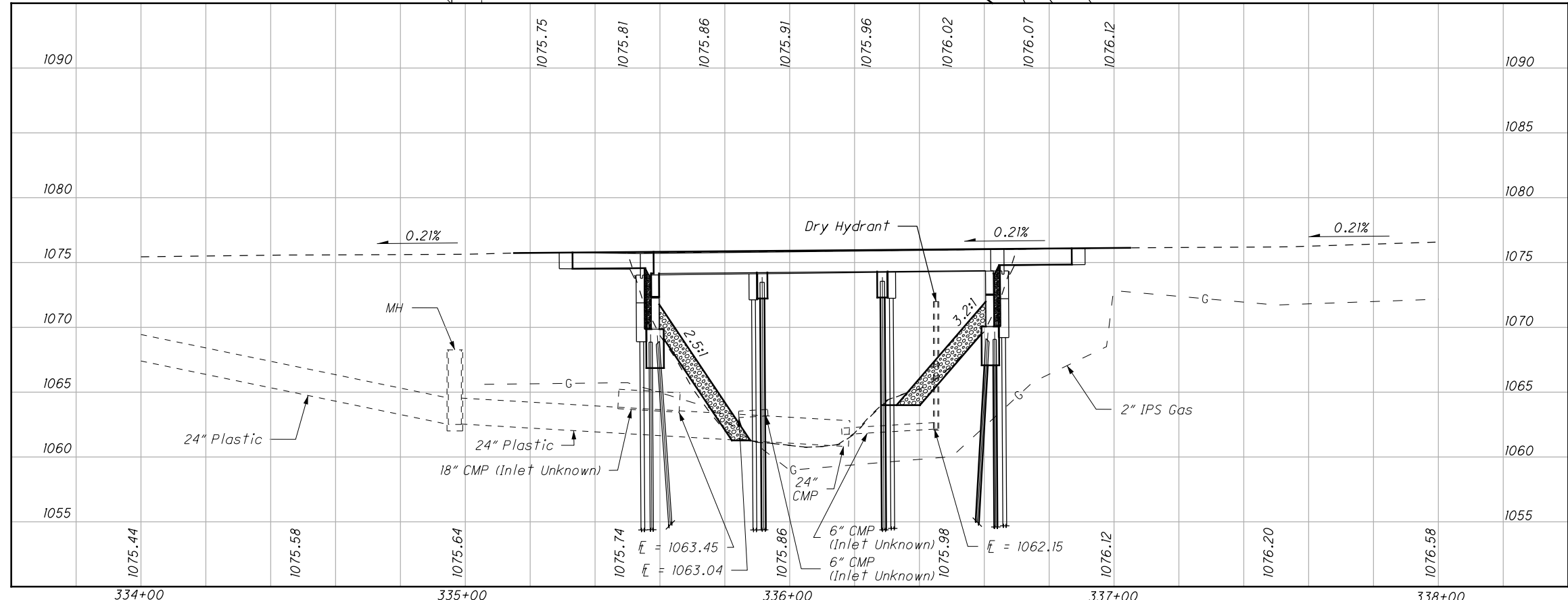
DAR-722-6.34



0 10 20 30 40
HORIZONTAL SCALE IN FEET



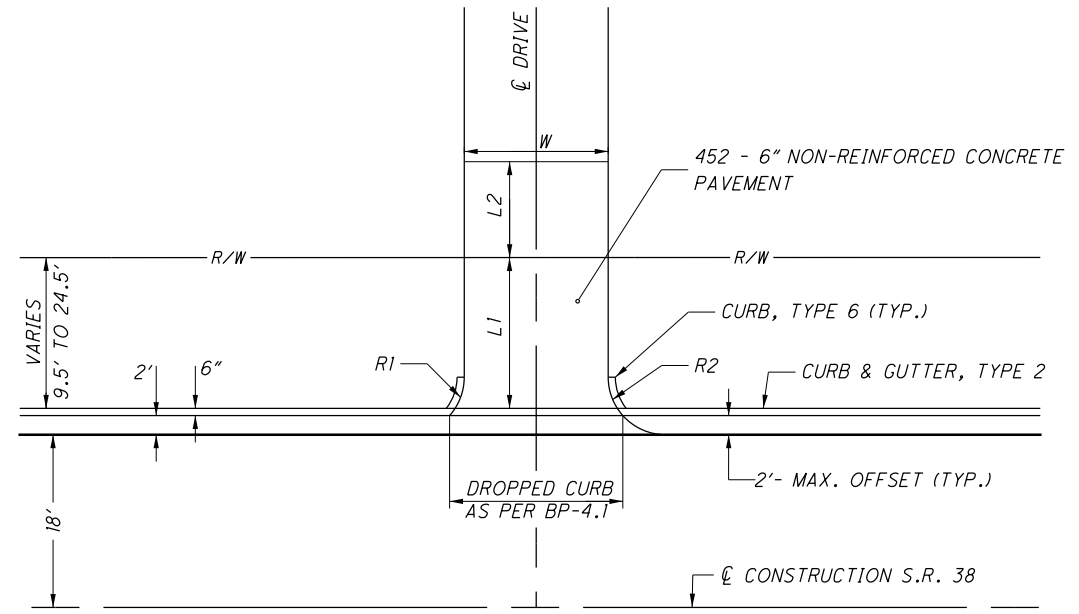
For Existing and Proposed Structure blocks see sheet 14.



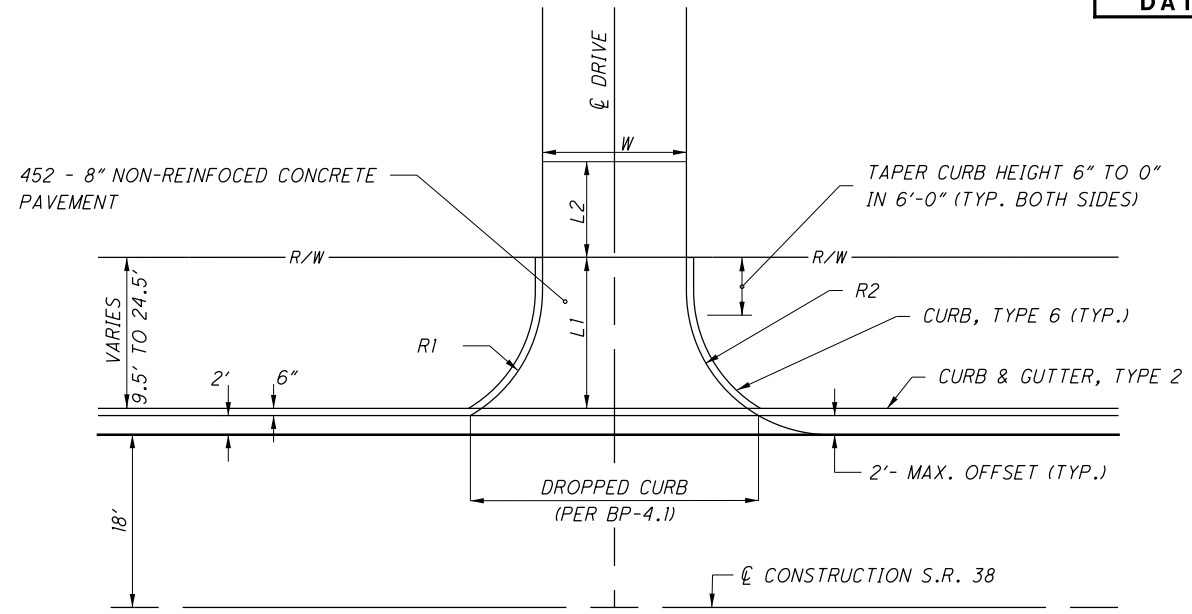
| REF NO. | STATION | | SIDE | QUANTITY | UNIT | TOTALS CARRIED TO GENERAL SUMMARY |
|--|----------|----------|--|----------|------|-----------------------------------|
| | FROM | TO | | | | |
| GR-1 | 334+02.5 | 335+49.5 | RT. | 1 | | |
| GR-2 | 334+62.3 | 335+64.5 | LT. | 1 | | |
| GR-3 | 336+54.6 | 337+77.2 | LT. | 1 | | |
| GR-4 | 336+69.2 | 337+67.6 | RT. | 1 | | |
| R-2 | 334+92 | 337+39 | LT. | 1 | | |
| R-3 | 335+07 | 337+28 | RT. | 1 | | |
| R-4 | 336+80 | 337+64 | RT. | 1 | | |
| RPC-1 | 335+49 | 335+98 | LT/RT | 1 | | |
| RPC-2 | 336+23 | 336+71 | LT/RT | 1 | | |
| WK-2 | 336+92 | 337+64 | RT. | 1 | | |
| TOTALS CARRIED TO GENERAL SUMMARY | | | | | | |
| 601 | | | ROCK CHANNEL PROTECTION TYPE C WITH FILTER | | CY | 250 |
| 608 | | | 4" CONCRETE WALK | | SF | 354 |
| 606 | | | BRIDGE TERMINAL ASSEMBLY, TYPE 4 | | EACH | 4 |
| 606 | | | ANCHOR ASSEMBLY, TYPE T | | EACH | 1 |
| 606 | | | ANCHOR ASSEMBLY, TYPE A | | EACH | 3 |
| 606 | | | GUARDRAIL TYPE 5 | | FT | 387.5 |
| 202 | | | GUARDRAIL REMOVED | | FT | 475 |
| 202 | | | WALK REMOVED | | SF | 405 |

PLAN AND PROFILE - S.R. 722
STA. 334+00 TO STA. 338+00

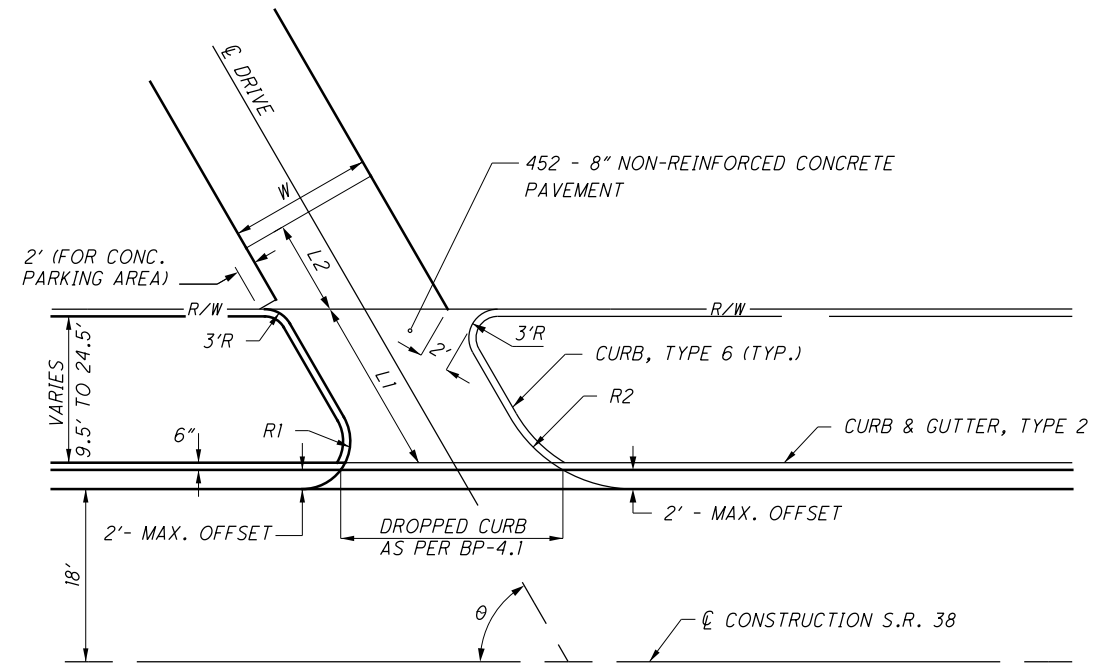
DAR-722-6.34



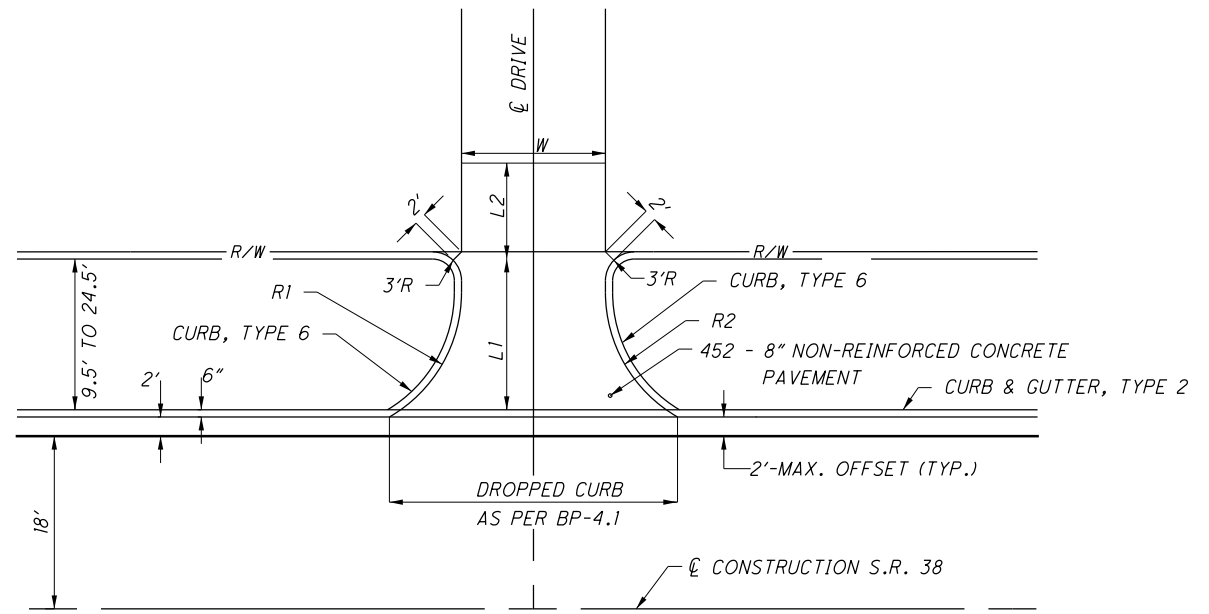
RESIDENTIAL DRIVES



COMMERCIAL DRIVES WITHOUT WRAP-AROUND CURB



SERVICE STATION DRIVES
FOR θ , SEE PLAN/PROFILE SHEETS.



COMMERCIAL DRIVES WITH WRAP-AROUND CURB

RESIDENTIAL DRIVES

- EXISTING AGGREGATE DRIVES
 - 452 - 6" NON-REINFORCED CONCRETE PAVEMENT (APRON)
 - 301 - 8" ASPHALT CONCRETE BASE, PG64-22
- EXISTING ASPHALT DRIVES
 - 452 - 6" NON-REINFORCED CONCRETE PAVEMENT (APRON)
 - 301 - 2" ASPHALT CONCRETE BASE, PG64-22
 - 407 - TACK COAT
 - 304 - 6" AGGREGATE BASE
- EXISTING CONCRETE DRIVES
 - 452 - 6" NON-REINFORCED CONCRETE PAVEMENT (APRON)

COMMERCIAL AND SERVICE STATION DRIVES

- EXISTING AGGREGATE DRIVE
 - 452 - 8" NON-REINFORCED CONCRETE PAVEMENT (APRON)
 - 304 - 10" AGGREGATE BASE
- EXISTING ASPHALT DRIVE
 - 452 - 8" NON-REINFORCED CONCRETE PAVEMENT (APRON)
 - 442 - 1/4" ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A, (448)
 - 442 - 1 3/4" ASPHALT CONCRETE, INTERMEDIATE COURSE, TYPE 1, (448)
 - 407 - TACK COAT
 - 304 - 8" AGGREGATE BASE
- EXISTING CONCRETE DRIVE
 - 452 - 8" NON-REINFORCED CONCRETE PAVEMENT (APRON)

SEE SHEET 41 FOR DRIVEWAY QUANTITIES.

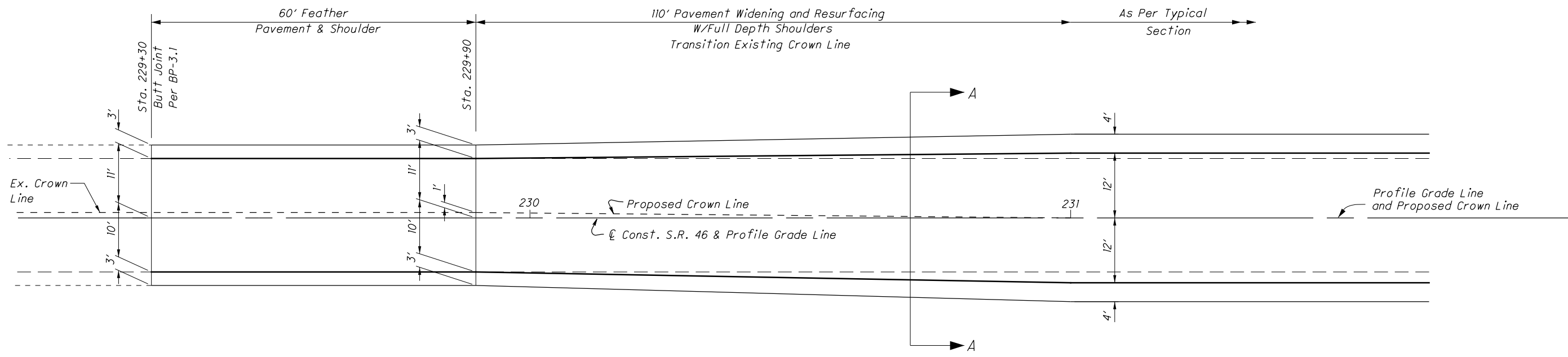
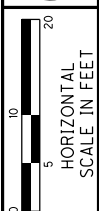
DRIVEWAY DETAILS

CAR-38-22.56

| SHEET NO. | REFERENCE NO. | STATION | SIDE | DRIVE TYPE | APRON LENGTH "L1" | DRIVEWAY LENGTH "L2" | WIDTH "W" | R1 (LEFT SIDE RADIUS OF DRIVE LOOKING FROM ☐) | R2 (RIGHT SIDE RADIUS OF DRIVE LOOKING FROM ☐) | 202 | 203 | | 301 | 304 | | | 407 | 442 | | 452 | | CALCULATED WSR CHECKED SJB | | |
|--|---------------|-------------------|------|------------|-------------------|----------------------|-----------|---|--|------------------|------------|------------|-----------------------------------|-------------------|-------------------|--------------------|-----------|--|---|-------------------------------------|-------------------------------------|-------------------------------------|------------|-------------|
| | | | | | | | | | | PAVEMENT REMOVED | EMBANKMENT | EXCAVATION | 8" ASPHALT CONCRETE BASE, PG64-22 | 6" AGGREGATE BASE | 8" AGGREGATE BASE | 10" AGGREGATE BASE | TACK COAT | 1 1/2" ASPHALT CONCRETE, INTERMEDIATE COURSE, 19MM, TYPE A (446) | 1/4" ASPHALT CONCRETE SURFACE COURSE, 4.5MM, TYPE A (446) | 6" NON-REINFORCED CONCRETE PAVEMENT | 8" NON-REINFORCED CONCRETE PAVEMENT | | | |
| | | | | | | | | | | SY | CY | CY | CY | CY | CY | CY | GAL | CY | CY | SY | SY | | | |
| 71 | DR-1 | 695+70 | RT | COMM. | 9.50 | 13.0 | 35.0 | 15.0 | 15.0 | 186.6 | | 8 | | | | | | | | | | 120.7 | | |
| 71 | DR-2 | 696+27.03 | RT | COMM. | 11.0 | 17.0 | 12.0 | 15.0 | 6.00 | | | 5 | 1.1 | | 5.2 | | 9.3 | | 0.8 | | | 17.1 | | |
| 71 | DR-3 | 696+62 | RT | COMM. | 9.50 | 15.0 | 35.0 | 15.0 | 15.0 | 217.7 | | 7 | | | | | | | | | | 218.9 | | |
| | | PARKING | RT | | | | | | | 111.7 | | 26 | 2.2 | | 10.6 | | 19.0 | | 1.6 | | | | | |
| 71 | DR-4 | 697+25 | LT | COMM. | 9.50 | 20.0 | 26.0 | 15.0 | 15.0 | | | 19 | 2.8 | | 12.8 | | 23.1 | | 2.0 | | | 31.8 | | |
| 71 | DR-5 | 697+68 | RT | COMM. | 9.50 | 10.0 | 35.0 | 15.0 | 15.0 | 141.7 | | 11 | | | | | | | | | | 85.6 | | |
| 71 | | PARKING TO 698+00 | RT | | | | | | | 26.1 | | 12 | | | | | | | | | | 26.1 | | |
| 74 | DR-1 | 10+75 | RT | COMM. | 17.5 | -- | 35.0 | 15.0 | 3.00 | 67.7 | | 4 | | | | | | | | | | 66.2 | | |
| | | 2ND ST. N.W. | | | | | | | | | | | | | | | | | | | | | | |
| 74 | DR-2 | 10+36 | LT | RES. | 17.0 | 23.3 | 8.00 | 4.00 | 6.00 | | | 15 | | 2.7 | | 6.4 | | 0.9 | | 30.0 | | | | |
| | | 2ND ST. N.W. | | | | | | | | | | | | | | | | | | | | | | |
| 74 | DR-3 | 698+87 | LT | COMM. | 9.50 | 28.0 | 25.0 | 15.0 | 15.0 | | 6 | 10 | 3.8 | 17.3 | | 31.1 | | 2.7 | | | | 32.0 | | |
| 74 | DR-4 | 699+96 | RT | RES. | 10.0 | 9.50 | 15.0 | 6.00 | 6.00 | | | 13 | 3.9 | | | | | | | 7.8 | | | | |
| | | PARKING | | | | | | | | 29.3 | | 5 | 3.1 | | | | | | | 1.6 | | | | |
| 74 | DR-5 | 700+77 | RT | COMM. | 9.50 | 5.00 | 35.0 | 15.0 | 15.0 | | | 16 | | | | | | | | | | 40.7 | | |
| | | PARKING | RT | | | | | | | | | 2 | | | | | | | | | | | | |
| 74 | DR-6 | 701+32 | RT | COMM. | 10.0 | 11.0 | 21.0 | -- | 6.00 | | | 6 | | | | | | | | | | 27.1 | | |
| 76 | DR-1 | 702+45 | RT | COMM. | 9.50 | 12.5 | 35.0 | 15.0 | 15.0 | 165.6 | 16 | | | | | | | | | | | 94.9 | | |
| | | PARKING | | | | | | | | 53.7 | 3 | | | | | | | | | | | 53.7 | | |
| 76 | DR-2 | 703+60 | RT | COMM. | 10.0 | 26.5 | 35.0 | 15.0 | 15.0 | | | 29 | 5.5 | | 24.8 | | 44.7 | | 3.9 | | | 51.8 | | |
| 76 | DR-3 | 706+69 | LT | RES. | 9.50 | 5.00 | 13.0 | 6.00 | 6.00 | | | 3 | 1.5 | | | | | | | 14.4 | | | | |
| 76 | DR-4 | 704+21 | LT | RES. | 9.50 | 5.00 | 21.0 | 6.00 | 6.00 | | 1 | 1 | 1.5 | | | | | | | 14.4 | | | | |
| 76 | DR-5 | 705+14 | LT | RES. | 9.50 | -- | 28.0 | 6.00 | 6.00 | | 4 | | | | | | | | | 29.1 | | | | |
| 77 | DR-1 | 706+39 | LT | RES. | 9.50 | 5.00 | 8.00 | 6.00 | 6.00 | | | 2 | 1.0 | | | | | | | 8.7 | | | | |
| 77 | DR-2 | 707+44 | LT | RES. | 9.50 | 1.00 | 12.0 | 6.00 | 6.00 | | | 5 | | 0.2 | | 0.5 | | 0.1 | | 13.1 | | | | |
| 77 | DR-3 | 708+05 | RT | COMM. | 9.50 | 9.50 | 35.0 | 15.0 | 15.0 | | | 22 | 1.0 | | 4.2 | | 7.6 | | 0.7 | | | 41.1 | | |
| | | PARKING | | | | | | | | | | 11 | 1.8 | | 8.0 | | 14.4 | | 1.3 | | | | | |
| 77 | DR-4 | 709+01 | RT | COMM. | 9.50 | 5.00 | 19.0 | 15.0 | 15.0 | | | 5 | 0.6 | | 2.8 | | 5.1 | | 0.4 | | | 24.0 | | |
| 77 | DR-5 | 709+81 | LT | RES. | 9.50 | 10.5 | 17.0 | 6.00 | 6.00 | | | 5 | | 1.8 | | 4.2 | | 0.6 | | 19.6 | | | | |
| | | PARKING | | | | | | | | | | 3 | | 3.7 | | 8.9 | | 1.2 | | | | | | |
| 77 | DR-6 | 10+51 | LT | COMM. | 17.0 | 17.5 | 16.0 | 15.0 | 2.30 | | | | | | | | | | | | | 35.6 | | |
| | | 5TH ST. N.W. | | | | | | | | | | | | | | | | | | | | | | |
| 77 | DR-7 | 10+67 | LT | RES. | 17.0 | -- | 6.00 | 2.30 | 6.00 | 35.6 | | | | | | | | | | 18.9 | | | | |
| | | 5TH ST. N.W. | | | | | | | | | | | | | | | | | | | | | | |
| 78 | DR-1 | 710+44 | LT | COMM. | 9.50 | 10.0 | 21.5 | 15.0 | 15.0 | | | 13 | 1.4 | | 6.7 | | 12.0 | | 1.0 | | | 29.1 | | |
| | | PARKING | | | | | | | | | | 17 | 3.1 | | 13.7 | | 24.7 | | 2.2 | | | | | |
| 78 | DR-2 | 711+08 | LT | RES. | 9.50 | 39.0 | 17.0 | 6.00 | 6.00 | | 2 | 7 | | 9.1 | | 21.9 | | 3.0 | | 20.2 | | | | |
| 78 | DR-3 | 711+58 | RT | COMM. | 9.50 | 6.00 | 35.0 | 15.0 | 15.0 | | | 15 | 1.4 | | 6.4 | | 11.5 | | 1.0 | | | 42.7 | | |
| | | PARKING | | | | | | | | | | 16 | 2.8 | | 12.8 | | 23.1 | | 2.0 | | | | | |
| 78 | DR-4 | 711+96 | LT | COMM. | 9.50 | 16.5 | 35.0 | 15.0 | 5.00 | | | 16 | 3.9 | | 17.9 | | 32.3 | | 2.8 | | | 40.9 | | |
| | | PARKING | | | | | | | | | | 14 | 2.2 | | 10.4 | | 18.7 | | 1.6 | | | | | |
| 78 | DR-5 | 712+53 | RT | COMM. | 9.50 | 3.00 | 35.0 | 15.0 | 15.0 | | | 15 | | | | | | | | | | 42.7 | | |
| | | PARKING | RT | | | | | | | | | 2 | | | | | | | | | | | | |
| 78 | DR-6 | 713+39 | LT | COMM. | 9.50 | 13.0 | 35.0 | 5.00 | 15.0 | | | 9 | 2.8 | | 12.8 | | 23.1 | | 2.0 | | | 36.4 | | |
| | | PARKING | LT | | | | | | | | | 1 | 0.1 | | 0.7 | | 1.3 | | 0.1 | | | | | |
| 78 | DR-7 | 713+18 | RT | COMM. | 9.50 | 3.00 | 35.0 | 15.0 | 15.0 | | | 15 | | | | | | | | | | 42.7 | | |
| | | PARKING | RT | | | | | | | | | 6 | | | | | | | | | | | | |
| 78 | DR-8 | 713+64 | LT | RES. | 9.50 | 20.0 | 14.0 | 6.00 | 6.00 | | 1 | 2 | 6.4 | | 4.4 | | 8.0 | | 0.7 | | 16.2 | | | |
| TOTALS CARRIED TO GENERAL SUMMARY | | | | | | | | | | 1036 | 33 | 393 | | 55 | 214 | | | | 351 | 33 | | | 194 | 1202 |

DRIVEWAY SUBSUMMARY

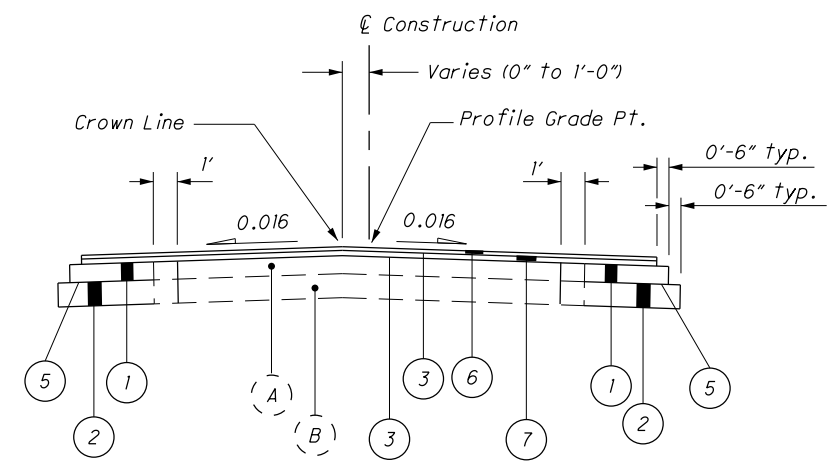
CAR -38 - 22.56



PAVEMENT TRANSITION DETAIL

LEGEND

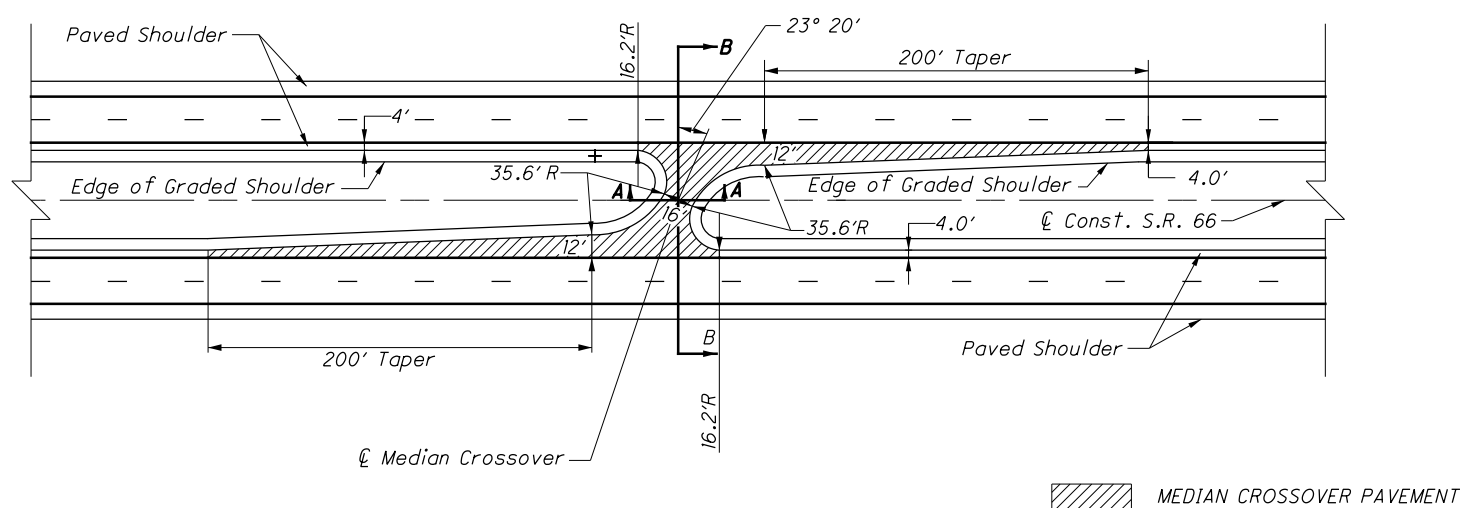
- (1) ITEM 301 - 5" ASPHALT CONCRETE BASE, PG64-22
- (2) ITEM 304 - 6" AGGREGATE BASE
- (3) ITEM 407 - TACK COAT (Applied at a rate of 0.075 gal/yd²)
- (4) NOT USED
- (5) ITEM 408 - PRIME COAT (Applied at a rate of 0.4 gal/yd²)
- (6) ITEM 441 - 1/4" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446) PG64-22
- (7) ITEM 441 - 3/4" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446)
- (A) ± 5" Asphalt
- (B) ± 8" Macadam Base



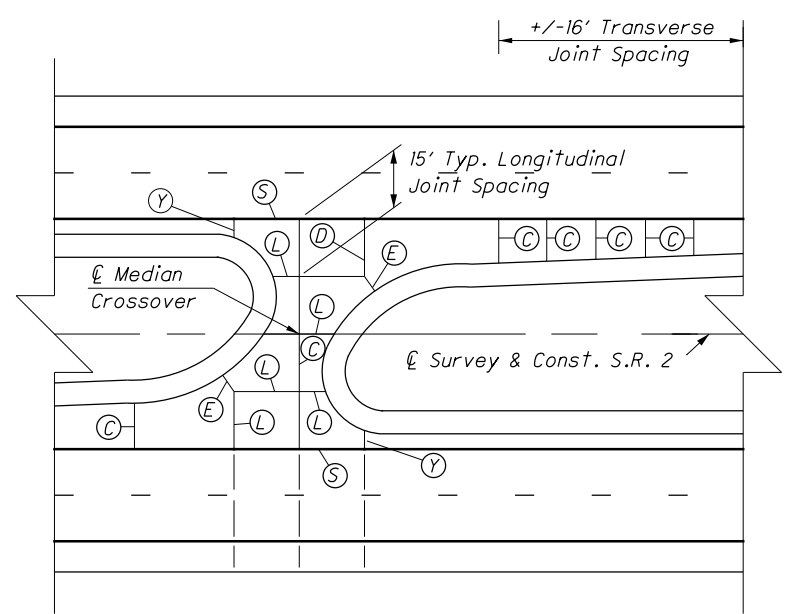
SECTION A-A

PAVEMENT TRANSITION DETAILS

PIC-46-27.22



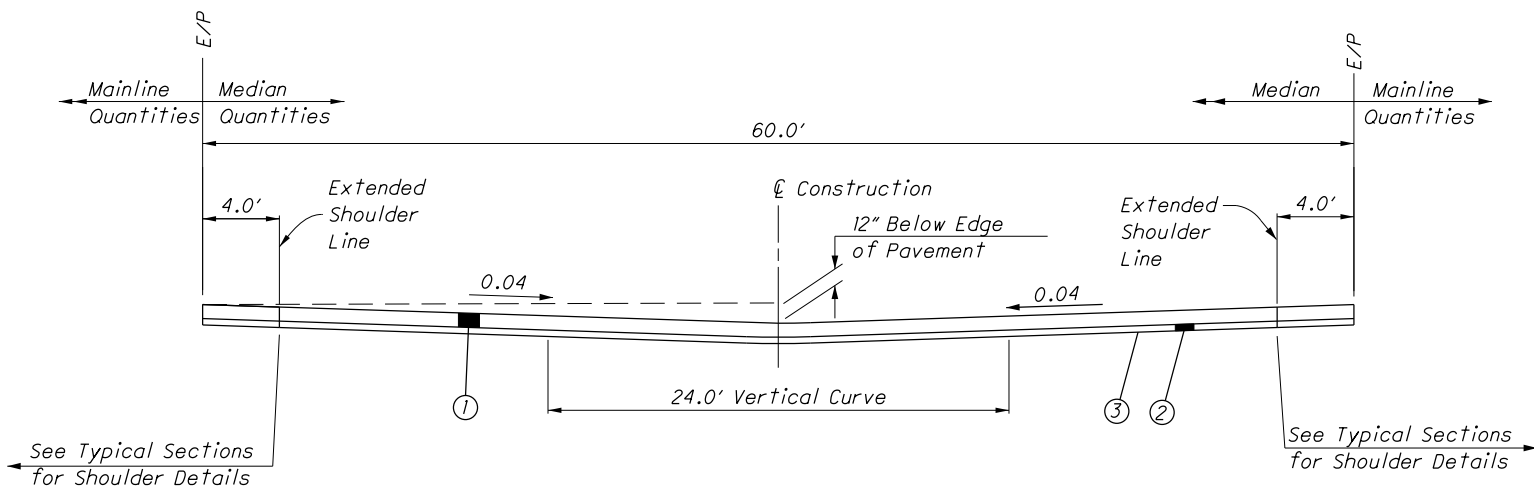
TYPICAL MEDIAN CROSSOVER DETAIL
Applies: Sta. 5+00 and Sta. 124+00



MEDIAN CROSSOVER JOINT DETAIL

*Align Transverse Joints in Median Crossover and Proposed Pavement.

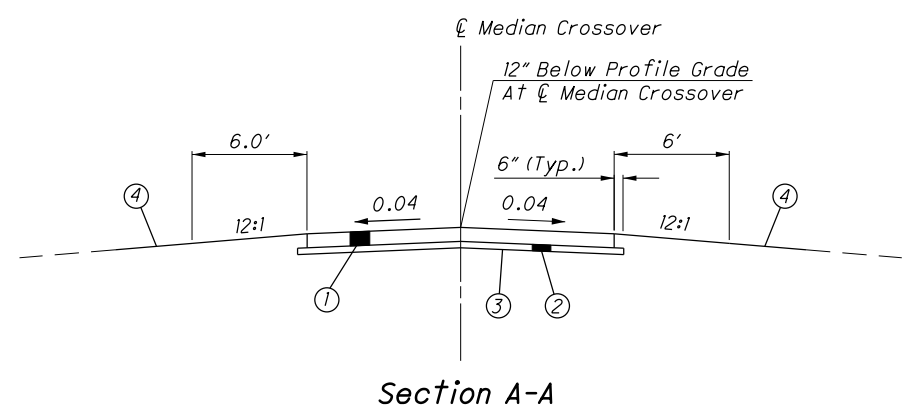
Note: The Above is A Suggested Joint Diagram. The Contractor May Submit an Alternate Joint Diagram to the Engineer for Approval.



Section B-B

LEGEND

- | | |
|--|--|
| ① ITEM 452 - 9" Non-Reinforced Concrete Pavement, Class QC1, As Per Plan | ⓐ Contraction Joint As Per BP-2.2 |
| ② ITEM 304 - 6" Aggregate Base | ⓓ Transverse Joint (With Dowels), As Per BP-2.2 |
| ③ ITEM 204 - Subgrade Compaction | ⓔ Expansion Joint (Without Dowels), As Per BP-2.2 |
| ④ ITEM 659 - Seeding And Mulching | Ⓢ Standard Longitudinal Joint, As Per BP-2.1 without Tie Bar 3 |
| | Ⓛ Longitudinal Joint, As Per BP-2.1 |
| | Ⓨ Contraction Joint (Type Y) As Per BP-2.5 |



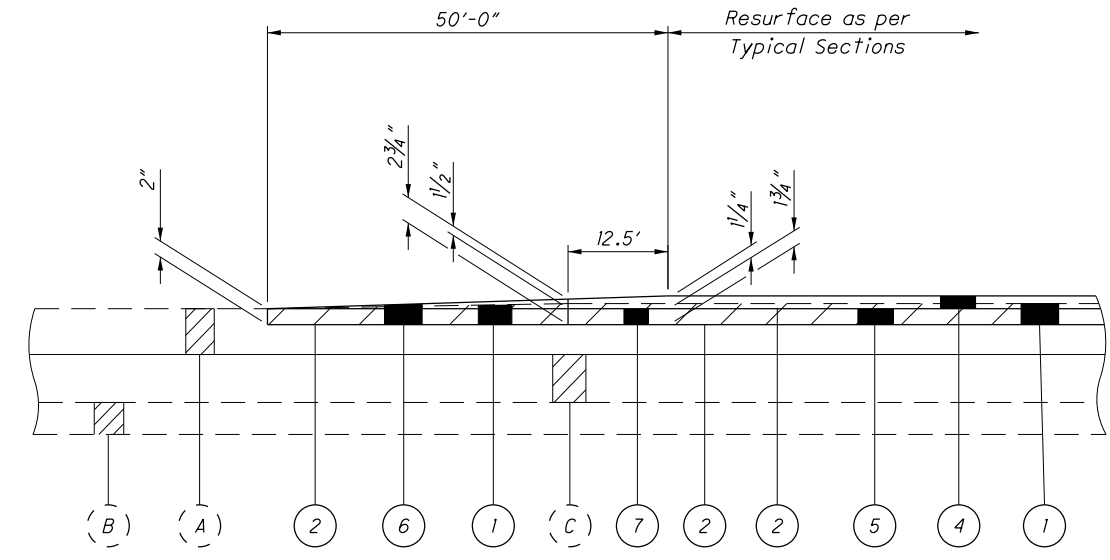
Section A-A

MEDIAN CROSSOVER DETAILS

OTT-66-4.33

PROPOSED LEGEND

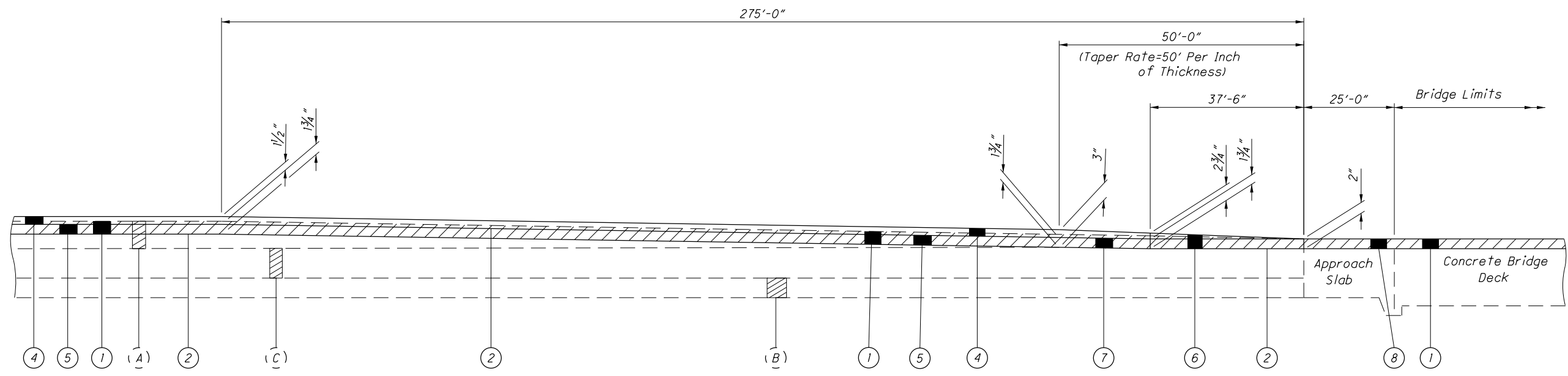
- ① ITEM 254 Pavement Planing, Asphalt Concrete (Depth As Shown)
- ② ITEM 407 Tack Coat
- ③ NOT USED
- ④ ITEM 441 1 1/4" Asphalt Concrete Surface Course, Type 1, (446) PG64-22
- ⑤ ITEM 441 1 3/4" Asphalt Concrete Intermediate Course, Type 1, (446)
- ⑥ ITEM 441 Var. Thickness Asphalt Concrete Surface Course, Type 1, (446) PG64-22
- ⑦ ITEM 441 Var. Thickness Asphalt Concrete Intermediate Course, Type 1, (446)
- ⑧ ITEM 848 1 3/4" Superplasticized Dense Concrete Overlay Using Hydrodemolition



EXISTING LEGEND

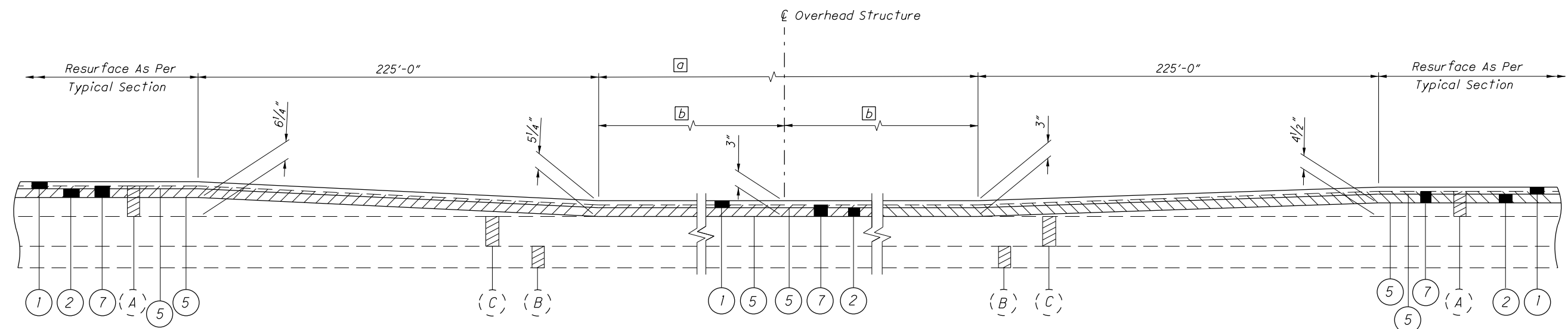
- (A) ± 6 1/2" Asphalt Concrete
 - (B) ± 6" Subbase
 - (C) ± 9" Reinforced Portland Cement Concrete Pavement
- = Item 254 Pavement Planing, Asphalt Concrete

PAVEMENT TRANSITION AT BEGIN/END PAVEMENT



TRANSITION AT STRUCTURES

DETAIL APPLIES AT:
VAN-277-0585 Lt. & Rt. (North & South End)
VAN-277-1041 Lt. & Rt. (South Only)
VAN-277-1246 Lt. & Rt. (North Only)

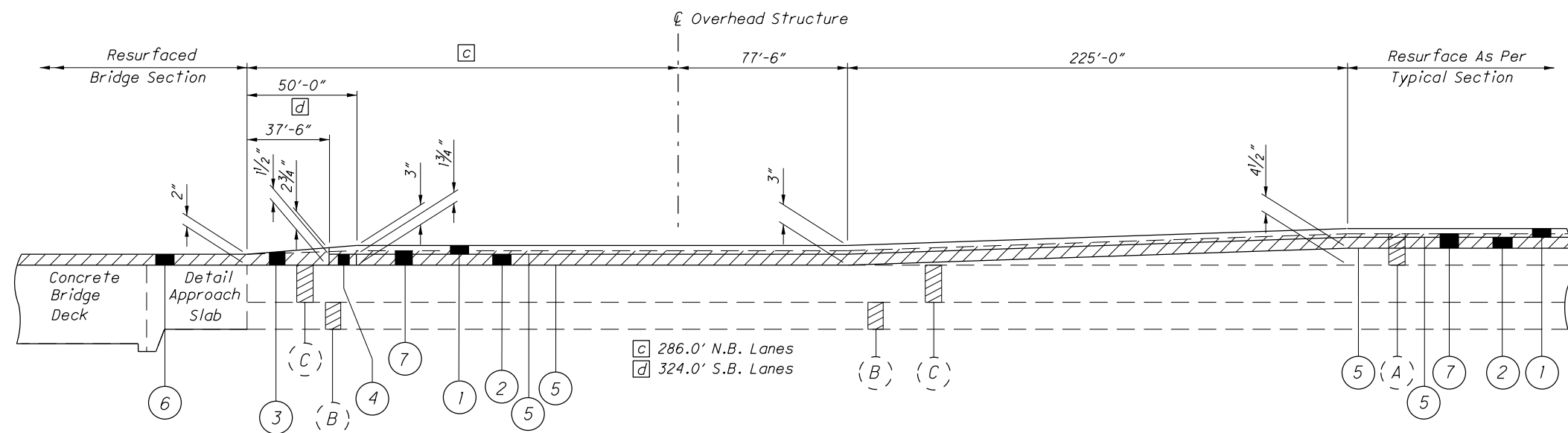


PAVEMENT TRANSITION AT OVERHEAD BRIDGE

DETAIL APPLIES AT STRUCTURES:

- VAN-277-0378 VAN-277-0776
- VAN-277-1234 VAN-277-0911
- VAN-277-1303 VAN-277-0493
- VAN-277-0633

| STRUCTURE | a | b |
|--------------|--------|-------|
| VAN-277-0378 | 144.0' | 72.0' |
| VAN-277-0493 | 160.0' | 80.0' |
| VAN-277-0633 | 166.0' | 83.0' |
| VAN-277-0776 | 165.0' | 82.5' |
| VAN-277-0911 | 179.0' | 89.5' |
| VAN-277-1234 | 155.0' | 77.5' |
| VAN-277-1383 | 156.0' | 78.0' |



PAVEMENT TRANSITION AT OVERHEAD AND MAINLINE BRIDGES

DETAIL APPLIES AT STRUCTURES:

- VAN-277-1047
- VAN-277-1041 (North Side Only)

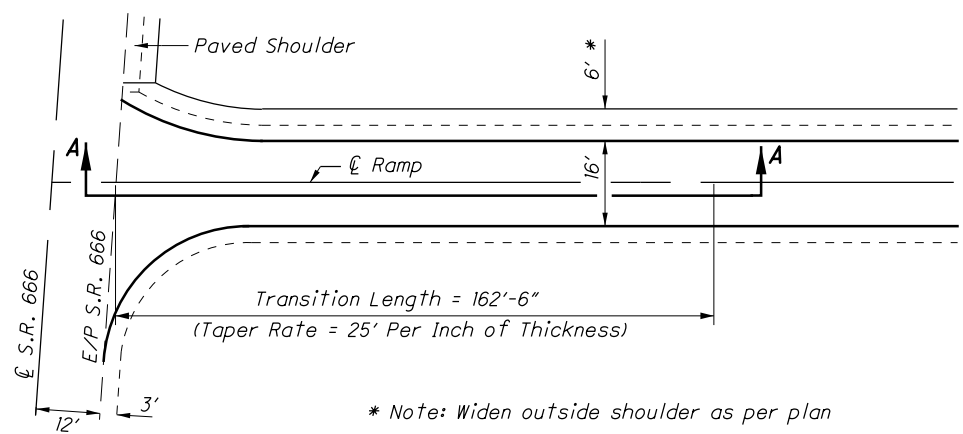
c 286.0' N.B. Lanes
d 324.0' S.B. Lanes

PROPOSED LEGEND

- 1 ITEM 441 1 1/4" Asphalt Concrete Surface Course, Type 1, (446), PG 64-22
- 2 ITEM 441 1 3/4" Asphalt Concrete Intermediate Course, Type 2, (446)
- 3 ITEM 441 Var. Thickness Asphalt Concrete Surface Course, Type 2, (446), PG 64-22
- 4 ITEM 441 Var. Thickness Asphalt Concrete Intermediate Course, Type 2, (446)
- 5 ITEM 407 Tack Coat
- 6 ITEM 848 Micro Silica Modified Concrete using Hydrodemolition (Thickness 1 1/4" Nominal)
- 7 ITEM 254 Pavement Planing, Asphalt Concrete (Depth = 2")

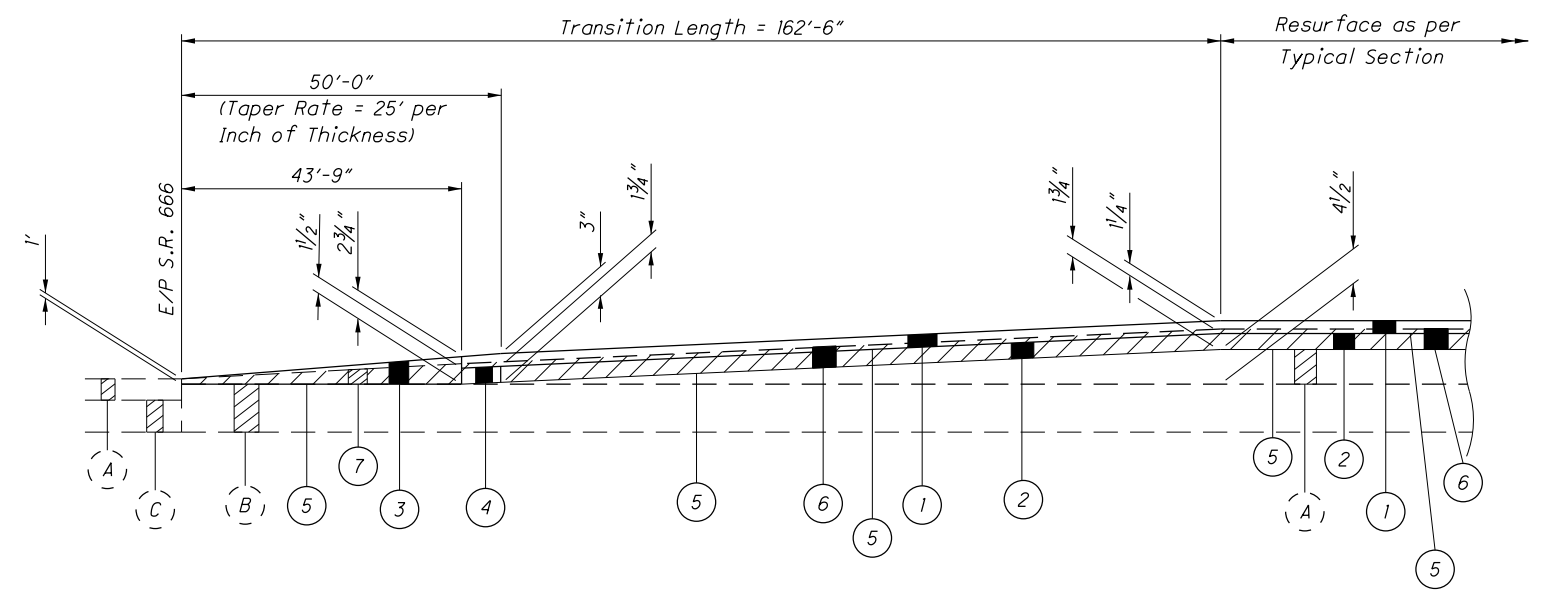
EXISTING LEGEND

- (A) ±6 1/2" Asphalt Concrete
- (B) ±6" Subbase
- (C) ±9" Reinforced Portland Cement Concrete Pavement
- ITEM 254 - Pavement Planing, Asphalt Concrete



* Note: Widen outside shoulder as per plan

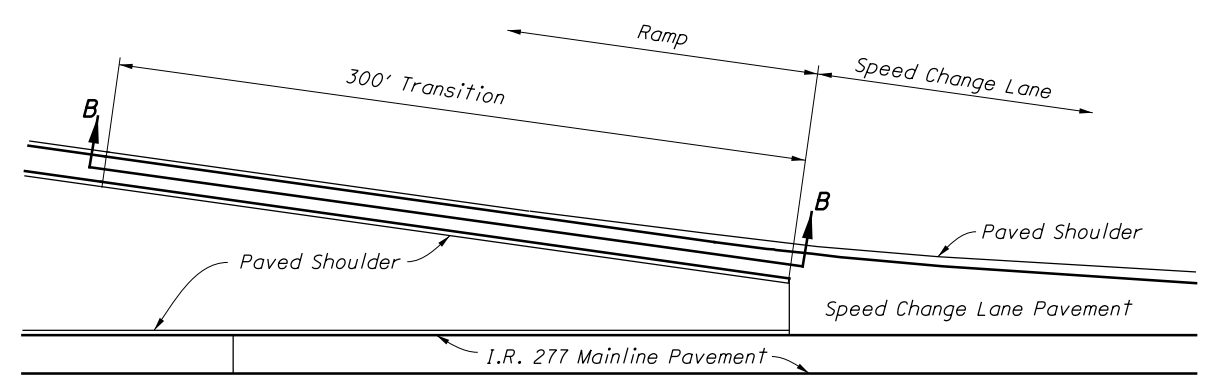
PLAN VIEW



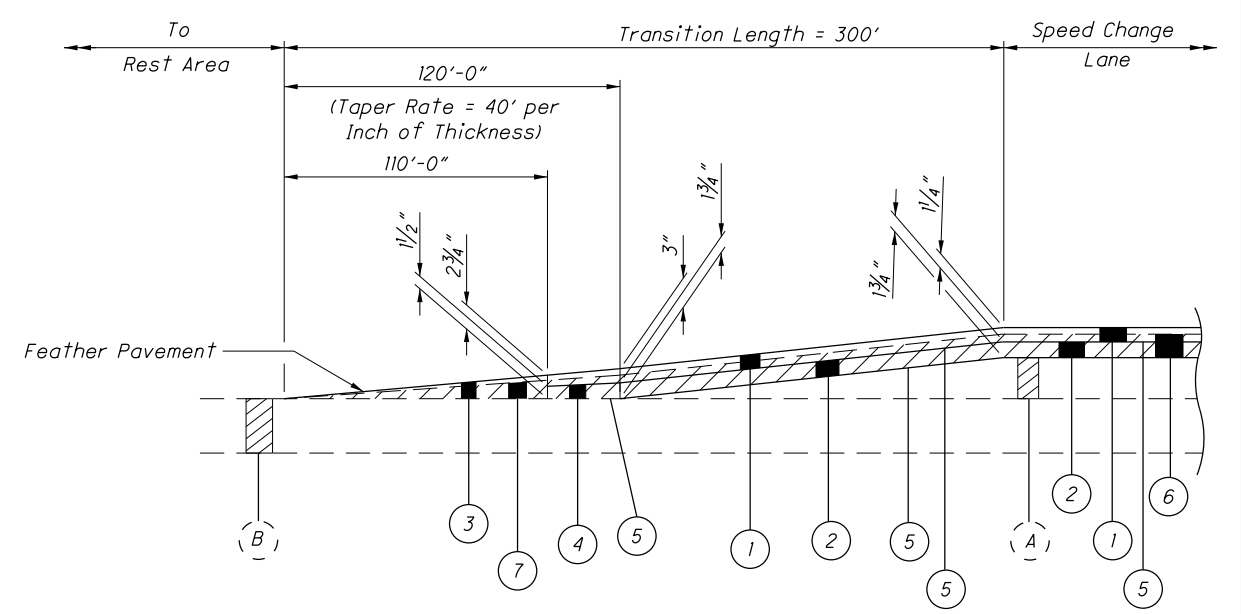
SECTION A-A

TRANSITION AT RAMP/CROSSROAD INTERSECTION

DETAIL APPLIES AT:
S.R. 666 Interchange



PLAN VIEW



SECTION B-B

TRANSITION AT REST AREA ENTRANCE/EXIT RAMP DETAIL

PROPOSED LEGEND

- | | |
|--|--|
| (1) ITEM 441 1 1/4" Asphalt Concrete Surface Course, Type 1, (446), PG 64-22 | (5) ITEM 407 Tack Coat |
| (2) ITEM 441 1 3/4" Asphalt Concrete Intermediate Course, Type 2, (446) | (6) ITEM 254 Pavement Planing, Asphalt Concrete (Depth = 2") |
| (3) ITEM 441 Var. Thickness Asphalt Concrete Surface Course, Type 1, (446), PG 64-22 | (7) ITEM 254 Pavement Planing, Asphalt Concrete (Var. Thickness) |
| (4) ITEM 441 Var. Thickness Asphalt Concrete Intermediate Course, Type 2, (446) | |

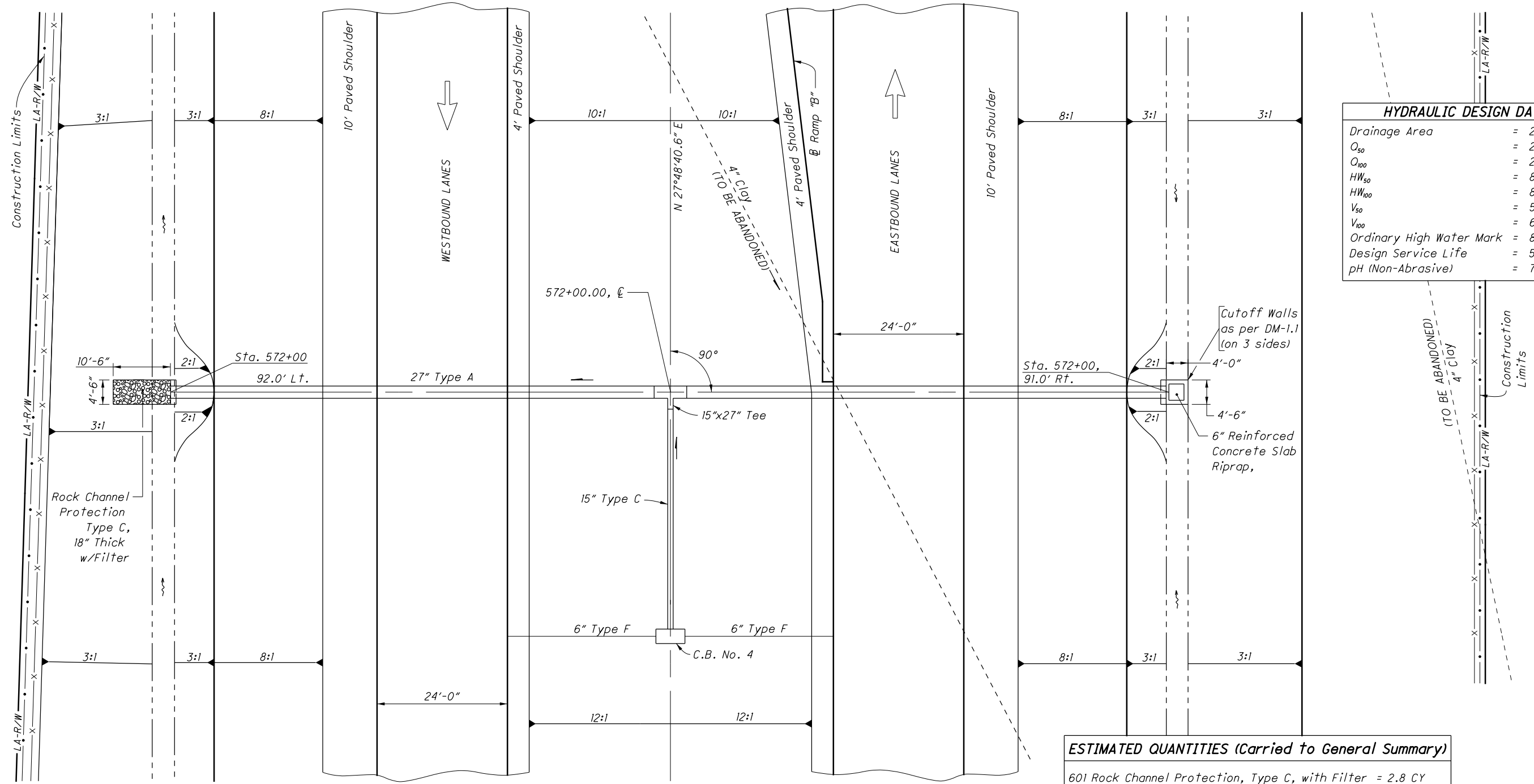
ITEM 254 Pavement Planing, Asphalt Concrete

EXISTING LEGEND

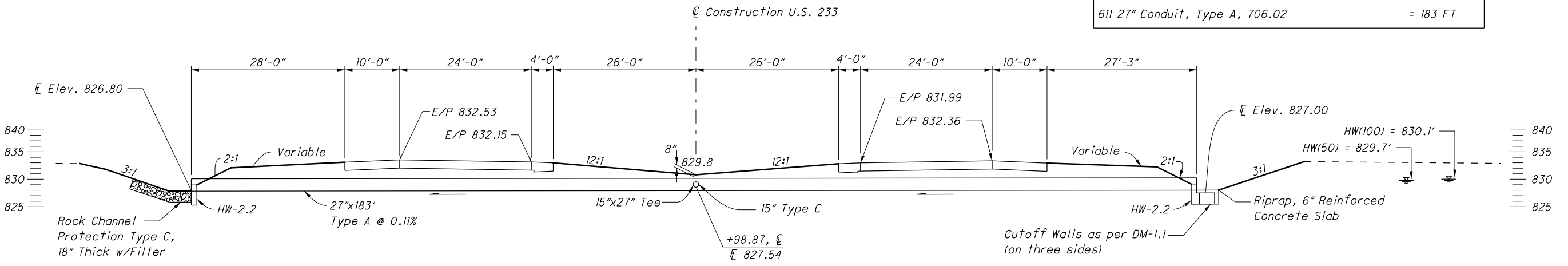
- (A) ±6 1/2" Asphalt Concrete
(B) ±9" Reinforced Portland Cement Concrete Pavement
(C) ±6" Aggregate Base



| HYDRAULIC DESIGN DATA | |
|--------------------------|-----------|
| Drainage Area | = 28 Ac. |
| Q_{50} | = 22 cfs |
| Q_{100} | = 25 cfs |
| HW_{50} | = 829.7' |
| HW_{100} | = 830.1' |
| V_{50} | = 5.6 fps |
| V_{100} | = 6.1 fps |
| Ordinary High Water Mark | = 827.3' |
| Design Service Life | = 50 Yr. |
| pH (Non-Abrasive) | = 7.8 |



| ESTIMATED QUANTITIES (Carried to General Summary) | |
|---|-----------|
| 601 Rock Channel Protection, Type C, with Filter | = 2.8 CY |
| 601 Riprap using 6" Reinforced | = 2.0 SY |
| 602 Concrete Masonry | = 1.15 CY |
| 611 27" Conduit, Type A, 706.02 | = 183 FT |



CALCULATED
DMK
CHECKED
CML

CULVERT DETAILS
U.S. 233 - STA. 572+00

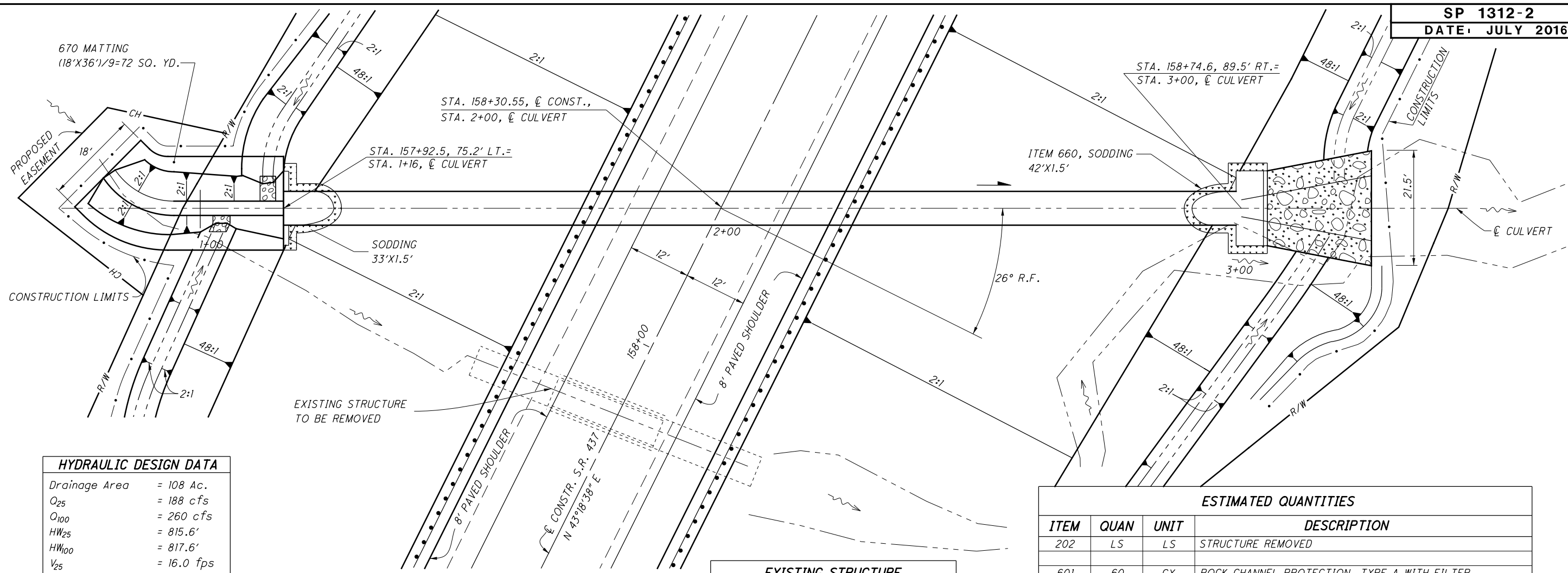
ALL-233-22.69



CALCULATED: JOH
CHECKED: JDH

CULVERT DETAIL
S.R. 437 STA. 158+30

PRE-437-2.65

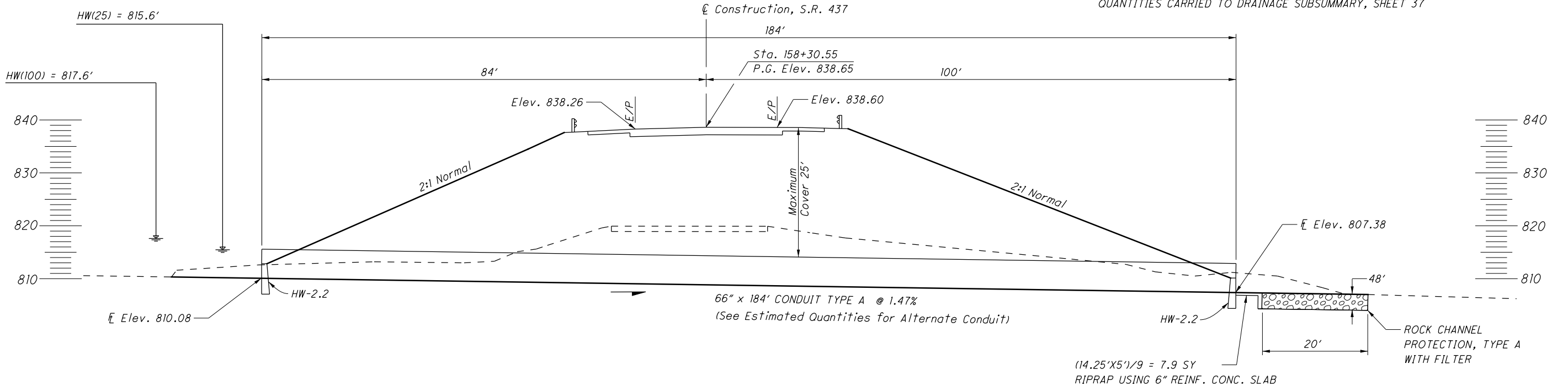


| HYDRAULIC DESIGN DATA | |
|--------------------------|------------|
| Drainage Area | = 108 Ac. |
| Q ₂₅ | = 188 cfs |
| Q ₁₀₀ | = 260 cfs |
| HW ₂₅ | = 815.6' |
| HW ₁₀₀ | = 817.6' |
| V ₂₅ | = 16.0 fps |
| V ₁₀₀ | = 18.5 fps |
| ORDINARY HIGH WATER MARK | = 810.3' |
| DESIGN SERVICE LIFE | = 75 YR |
| pH ABRASIVE | = 7.5 |

EXISTING STRUCTURE
 TYPE: STONE ARCH W/72" CMP EXTENSIONS
 SIZE: 6'X5'X58" ARCH
 SKEW: 4°14' R.F.
 ALIGNMENT: TANGENT
 DATE BUILT: 1908
 CONDITION: POOR

| ESTIMATED QUANTITIES | | | |
|----------------------|------|------|--|
| ITEM | QUAN | UNIT | DESCRIPTION |
| 202 | LS | LS | STRUCTURE REMOVED |
| 601 | 60 | CY | ROCK CHANNEL PROTECTION, TYPE A WITH FILTER |
| 601 | 8 | SY | RIPRAP |
| 602 | 5.9 | CY | CONCRETE MASONRY |
| 611 | 184 | FT | 66" CONDUIT, TYPE A, 706.02; OR 78", 707.01 (0.168) ALUMINIZED, 707.02 (0.138) ALUMINIZED, 707.03 (0.109) W/CFP, 707.04 (1") (0.079), 707.05 (0.109), 707.07 (0.109) |
| 660 | 13 | SY | SODDING |
| 670 | 72 | SY | DITCH EROSION PROTECTION MAT TYPE C |

QUANTITIES CARRIED TO DRAINAGE SUBSUMMARY, SHEET 37





0 5 10 20
HORIZONTAL
SCALE IN FEET

CALCULATED
MRV
CHECKED
MLC

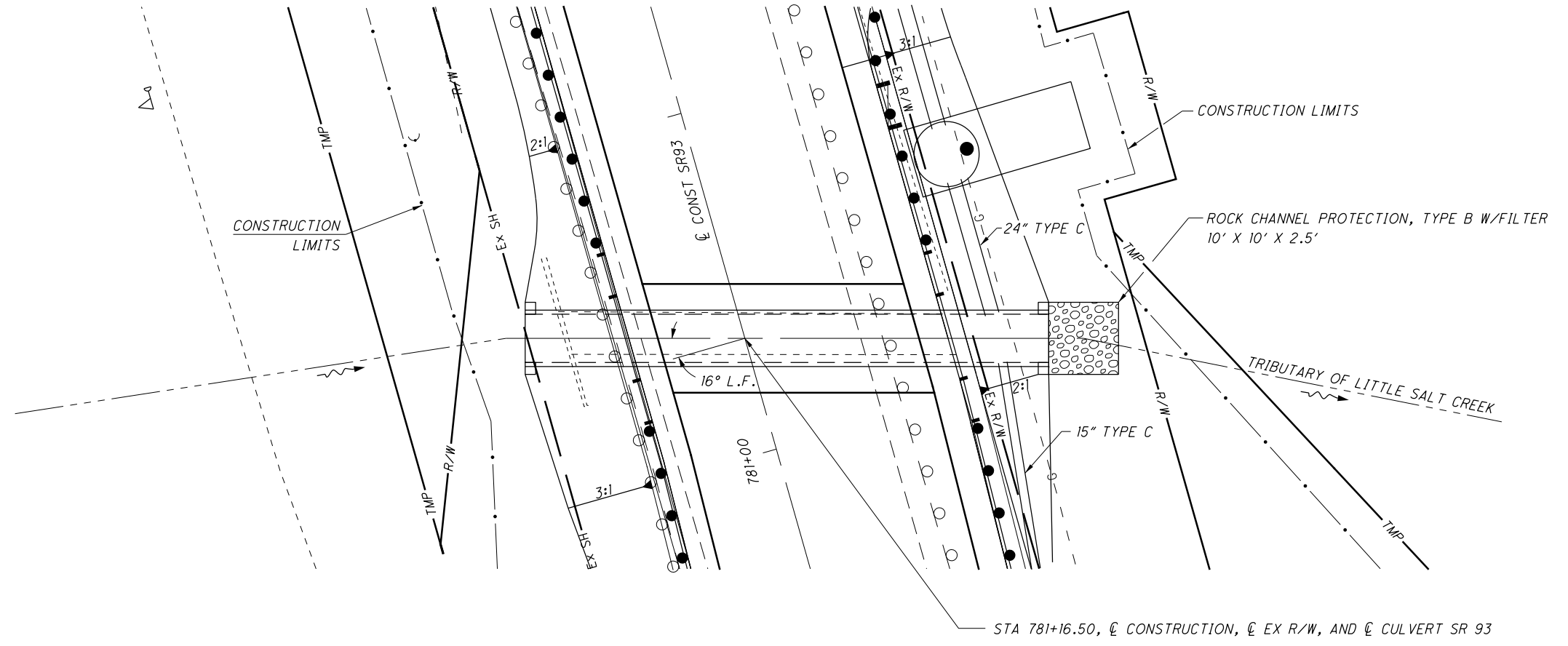
CULVERT DETAIL
STA 781+16.50

JAC-93-14.35

EXISTING STRUCTURE
TYPE: 48" AND 54" CORRUGATED METAL PIPES
SKEW: 16° L.F.
ALIGNMENT: TANGENT

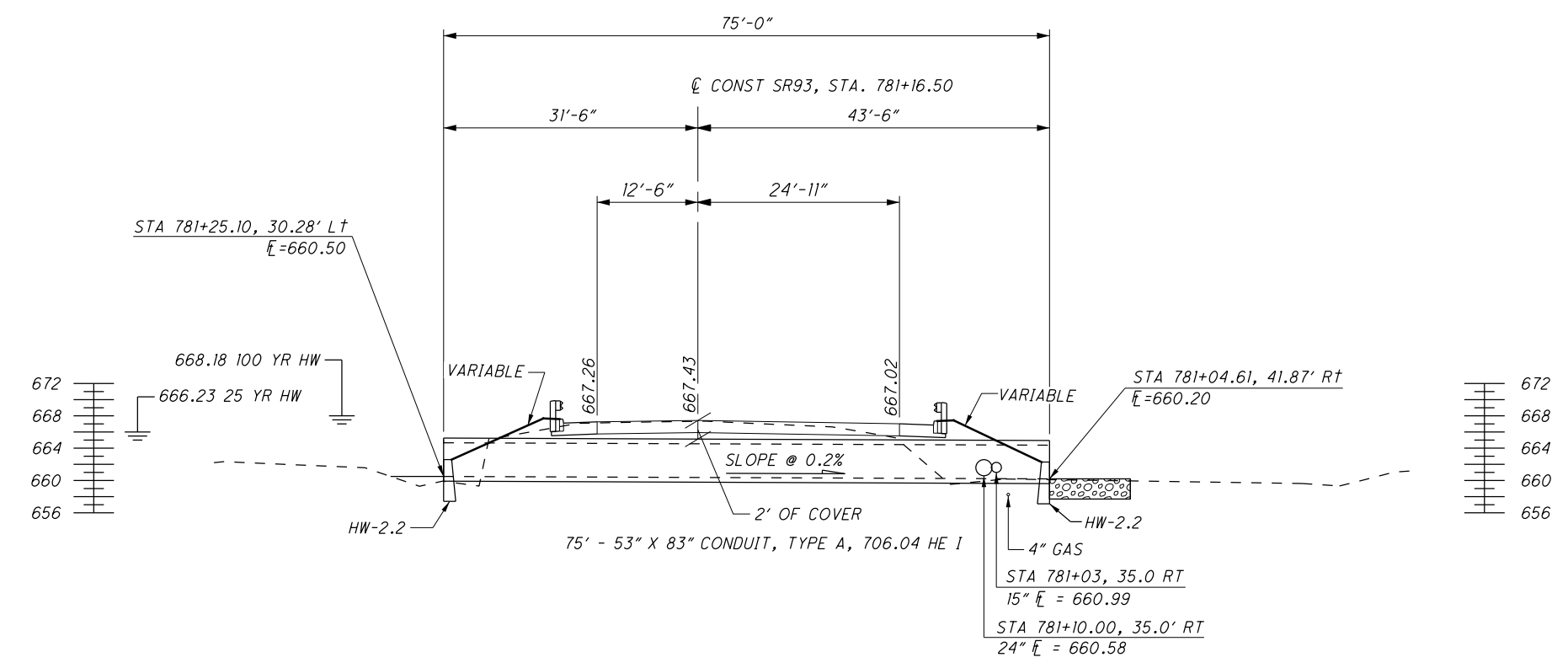
PROPOSED STRUCTURE
TYPE: 53"X83" ELLIPTICAL CONCRETE PIPE
SKEW: 16° L.F.
ALIGNMENT: TANGENT

HYDRAULIC DESIGN DATA
DRAINAGE AREA: 344 ACRES
Q(25): 230 CFS
HW(25): 666.24 FT
V(25): 11 FT/S
Q(100): 325 CFS
HW(100): 668.81 FT
V(100): 13 FT/S
ORDINARY HIGH WATER MARK: 661.0'
DESIGN SERVICE LIFE: 50 YRS
pH: 6.8
NON-ABRASIVE



ESTIMATED QUANTITIES CARRIED TO GENERAL SUMMARY

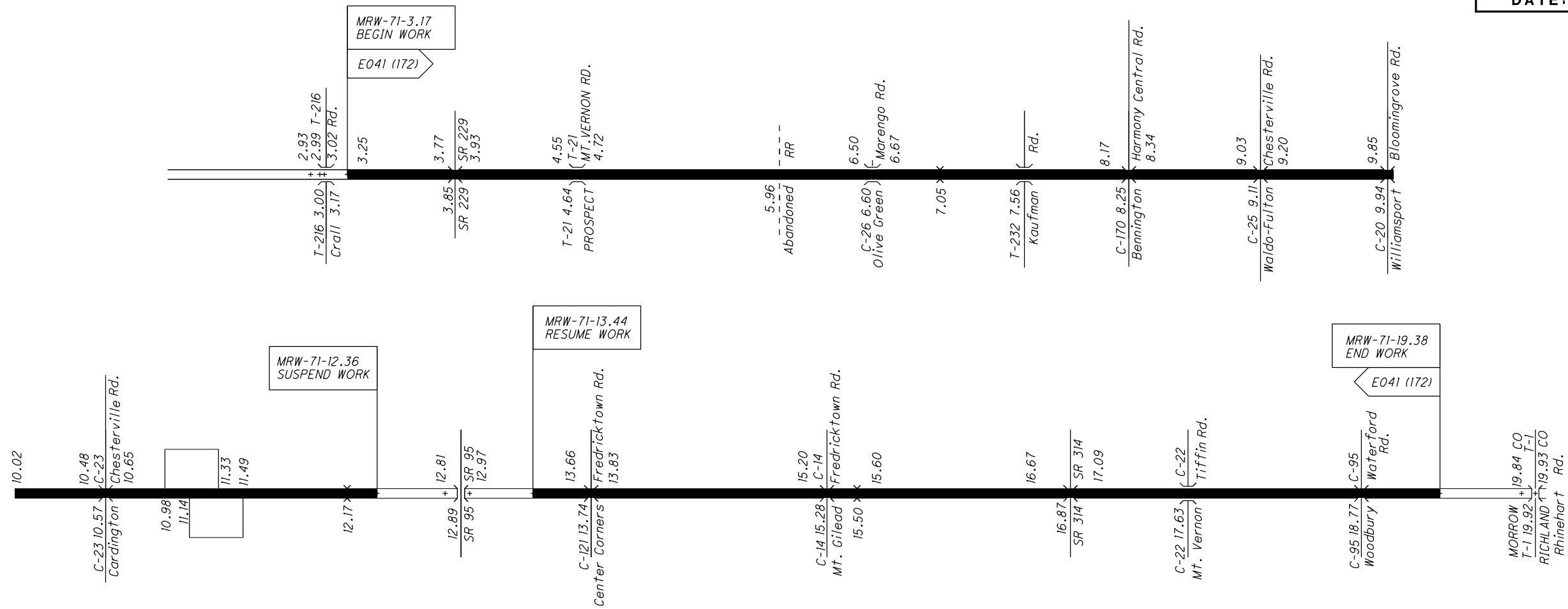
| ITEM | QUANTITY | UNIT | DESCRIPTION |
|------|----------|------|---|
| 601 | 9 | CY | ROCK CHANNEL PROTECTION, TYPE B, W/FILTER |
| 602 | 3.3 | CY | CONCRETE MASONRY |
| 611 | 75 | FT | 53" X 83" CONDUIT, TYPE A, 706.04 HE I |



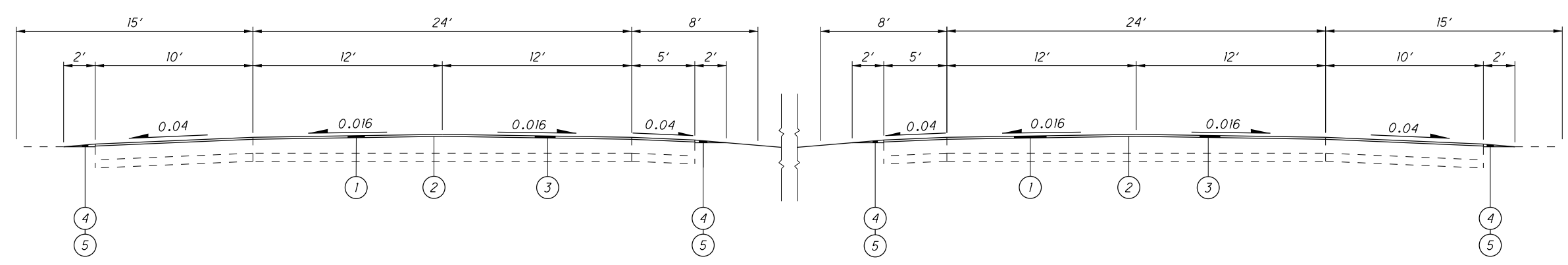
| REFERENCE NUMBER | SHEET No. | SIDE | ROADWAY | STATION TO STATION | 625 | | | | | | | | | | | | | | | | | Ground Rod | Structure Grounding System | Plastic Caution Tape | |
|---|-----------|------|---------|--------------------|---|--|----------------------------|----------------------------|--------------------------------------|-----------------------|-------------|---------------------|---------------------|-------------------------------------|---------------------------------|---|------------------------------|--------------------------------|----------------------------|-------------------------------|----------------------------|------------|----------------------------|----------------------|--|
| | | | | | Luminaire, Style B, Type II, 200 W H.P.S., 480V | Luminaire, Style B, Type III, 200 W H.P.S., 480V | Light Pole, Design AT15B40 | Light Pole, Design AT20B40 | Light Pole Foundation, 24" x 8' Deep | Pull Box, 725.08, 18" | Trench, 24" | Conduit, 2", 725.04 | Conduit, 3", 725.04 | No. 4 AWG 5000 V Distribution Cable | No. 10 AWG Pole & Bracket Cable | 1/2" Duct Cable W/3 No. 4 AWG 5000 V Cables | Connection, Fused Pull-Apart | Connection, Unfused Pull-Apart | Connection, Unfused Bolted | Connection, Unfused Permanent | Power Service, As Per Plan | | | | |
| | | | | | EACH | EACH | EACH | EACH | EACH | FT | FT | FT | FT | FT | FT | EACH | EACH | EACH | EACH | EACH | EACH | EACH | EACH | FOOT | |
| SHEET 269 | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 | 269 | L† | US-44 | 346+82 | | 1 | | 1 | 1 | | | | | | 124 | 2 | | | | | | | | | |
| 2 | 269 | L† | US-44 | 346+82 to 349+44 | | | | | | 262 | | | | | | | | | | 1 | | | | 262 | |
| 3 | 269 | L† | US-44 | 349+44 | | | | | | | | | | | | | | | | | | | | | |
| 4 | 269 | L† | US-44 | 349+44 to 349+73 | | | | | | | | | | | | | | | | | | | | | |
| 5 | 269 | L† | US-44 | 349+73 | | | | | | | | | | | | | | | | | | | | | |
| 6 | 269 | L† | US-44 | 349+73 to 350+12 | | | | | | 39 | | | | | | | | | | | | | | 39 | |
| 7 | 269 | L† | US-44 | 350+12 | | 1 | | 1 | 1 | | | | | | 124 | 2 | | | | | | | | | |
| 8 | 269 | L† | US-44 | 350+12 to 353+17 | | | | | | 305 | | | | | | | | | | | | | | 305 | |
| 9 | 269 | L† | US-44 | 353+17 | | 1 | | 1 | 1 | | | | | | 124 | 2 | | | | | | | | | |
| 10 | 269 | L/R | 44/D | 353+17 to 554+20 | | | | | | 212 | | | 146 | | | | | | | | | | | 212 | |
| 11 | 269 | R† | US-44 | 347+89 | | 1 | | 1 | 1 | | | | | | 124 | 2 | | | | | | | | | |
| 12 | 269 | R† | US-44 | 347+89 to 348+51 | | | | | | 62 | | | | | | | | | | | | | | 62 | |
| 13 | 269 | R† | US-44 | 348+51 | | | | | | | | | | | | | | | | | | | | | |
| 14 | 269 | R† | US-44 | 348+51 to 348+80 | | | | | | | | | | | | | | | | | | | | | |
| 15 | 269 | R† | US-44 | 348+80 | | | | | | | | | | | | | | | | | | | | | |
| 16 | 269 | R† | 44/D | 348+80 to 554+20 | | | | | | 540 | | | | | | | | | | | | | | 540 | |
| 17 | 269 | R† | RAMP D | 554+20 | | 1 | | 1 | 1 | | | | | | 124 | 1 | 1 | 2 | | | | | | | |
| 18 | 269 | R† | RAMP D | 554+20 to 556+15 | | | | | | 195 | | | | | | | | | | | | | | 195 | |
| 19 | 269 | R† | RAMP D | 556+15 | | 1 | | 1 | 1 | | | | | | 114 | 1 | 1 | 2 | | | | | | | |
| 20 | 269 | R† | 44/D | 355+62 to 556+15 | | | | | | 85 | | | 39 | | | | | | | | | | | 85 | |
| 21 | 269 | R† | US-44 | 355+62 | | 1 | | 1 | 1 | | | | | | 124 | 2 | | | | | | | | | |
| 22 | 269 | R† | RAMP D | 556+15 to 560+30 | | | | | | 415 | | | | | | | | | | | | | | 415 | |
| SHEET 270 | | | | | | | | | | | | | | | | | | | | | | | | | |
| 23 | 270 | R† | SR-402 | 54+90 | | 1 | | 1 | 1 | | | | | | 114 | 2 | | | | | | | | | |
| 24 | 270 | L/R | SR-402 | 53+88 to 54+90 | | | | | | 188 | | | 97 | | | | | | | | | | | 188 | |
| 25 | 270 | L† | SR-402 | 53+88 | | 1 | | 1 | 1 | | | | | | 124 | 2 | | | | | | | | | |
| 26 | 270 | L† | SR-402 | 51+32 to 53+88 | | | | | | 256 | | | | | | | | | | | | | | 256 | |
| 27 | 270 | L† | SR-402 | 51+32 | | | | | | | | | | | | | | | | | | | | | |
| 28 | 270 | L† | SR-402 | 48+78 to 51+32 | | | | | | 20 | 254 | | 792 | | | | | | | | | | | 20 | |
| 29 | 270 | L† | SR-402 | 48+78 | | | | | | | | | | | | | | | | | | | | | |
| 30 | 270 | L† | 128/402 | 45+10 to 48+78 | | | | | | 368 | | | 80 | | | | | | | | | | | 368 | |
| 31 | 270 | L/R | D/128 | 560+30 to 45+20 | | | | | | 660 | | | | | | | | | | | | | | 660 | |
| 32 | 270 | R† | SR-402 | 46+22 | | 1 | | 1 | 1 | | | | | | 114 | 2 | | | | | | | | | |
| 33 | 270 | L/R | 128/402 | 45+10 to 46+22 | | | | | | 170 | | | 115 | | | | | | | | | | | 170 | |
| 34 | 270 | R† | 128/C | 45+20 to 475+86 | | | | | | 1030 | | | | | | | | | | | | | | 1030 | |
| 35 | 270 | L† | CR-128 | 45+10 | | 1 | | 1 | 1 | | | | | | 114 | 1 | 1 | 2 | | | | | | | |
| 36 | 270 | L† | CR-128 | 44+10 to 45+10 | | | | | | 100 | | | | | | | | | | | | | | 100 | |
| 37 | 270 | L† | CR-128 | 44+10 | | | | | | | | | | | | | | | | | | | | | |
| 38 | 270 | L† | CR-128 | 44+10 to 45+20 | | | | | | 110 | | | | | | | | | | | | | | 110 | |
| 39 | 270 | L/R | CR-128 | 44+10 to 45+20 | | | | | | 159 | | | 49 | | | | | | | | | | | 159 | |
| 40 | 270 | L† | RAMP B | 275+49 | | 1 | | 1 | 1 | | | | | | 114 | 2 | | | | | | | | | |
| 41 | 270 | L† | USR-44 | 357+98 | | | | | | | | | | | | | | | | | | | | | |
| 42 | 270 | L† | RAMP B | 275+49 to 276+01 | | | | | | 52 | | | | | | | | | | | | | | 52 | |
| 43 | 270 | L/R | B/44 | 275+49 to 357+98 | | | | | | 83 | | | 83 | | | | | | | | | | | 83 | |
| SUBTOTALS CARRIED TO GENERAL SUMMARY | | | | | 6 | 7 | 5 | 8 | 13 | 6 | 5311 | 312 | 609 | 1026 | 1562 | 5471 | 23 | 3 | 6 | 12 | 1 | 13 | 1 | 5311 | |

LIGHTING PLAN SUB-SUMMARY

CHP - 44 - 11.29



*** NOTES**
MAINTAIN THE EXISTING PAVEMENT
CROSS SLOPE. SHOULDER WIDTH
MAY VARY NEAR EXISTING RAMPS
AND CROSSOVERS.



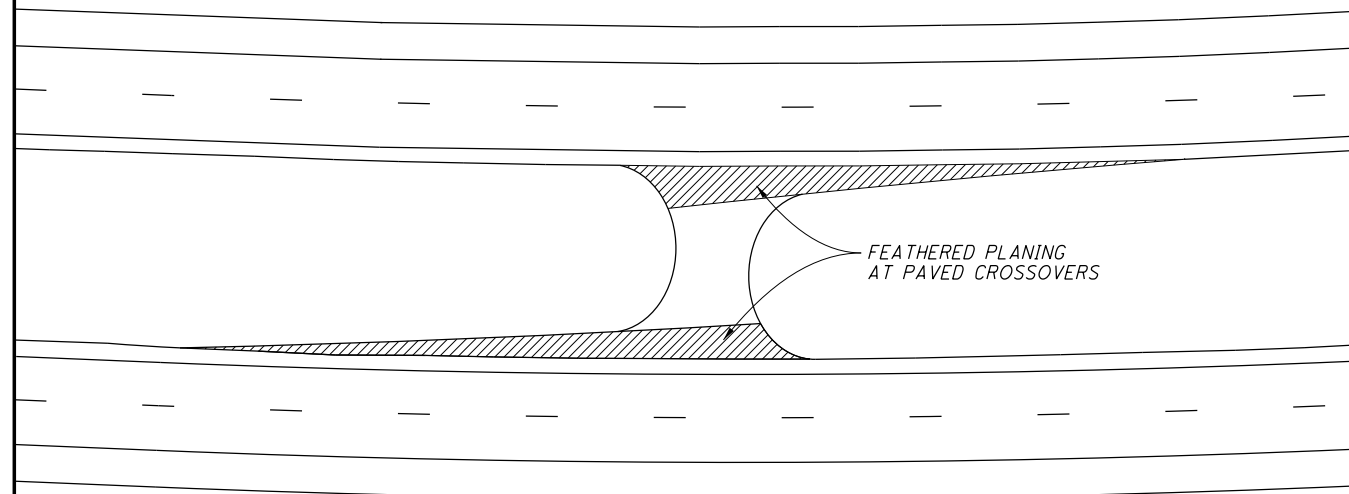
LEGEND

- ① ITEM 254 - 1/2" PAVEMENT PLANING, ASPHALT CONCRETE
- ② ITEM 407 - TACK COAT
- ③ ITEM 441 - 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), PG 64-22
- ④ ITEM 408 - PRIME COAT @ 0.4 GALLONS PER SQ. YD.
- ⑤ ITEM 617 - COMPACTED AGGREGATE

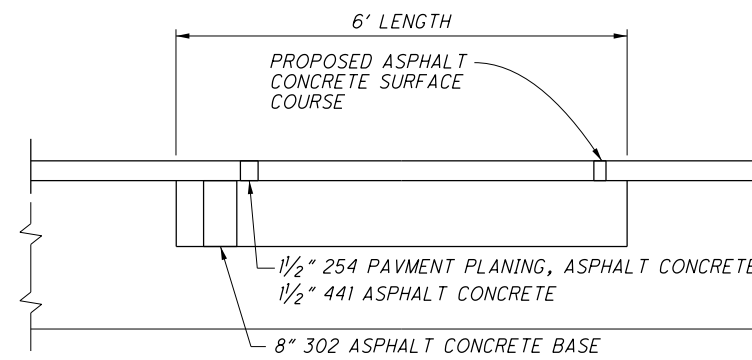
RESURFACING

MRW - 71 - 3.17

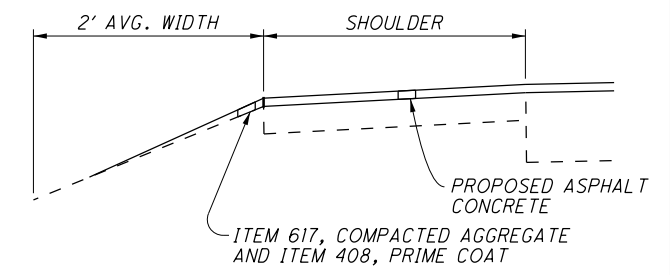
CROSSOVER DETAIL



PARTIAL DEPTH PAVEMENT REPAIR AS PER PLAN



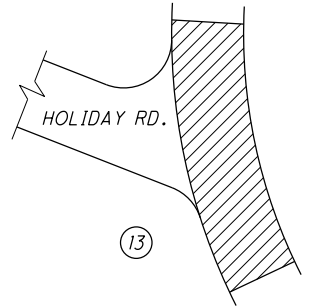
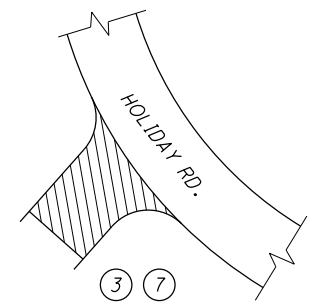
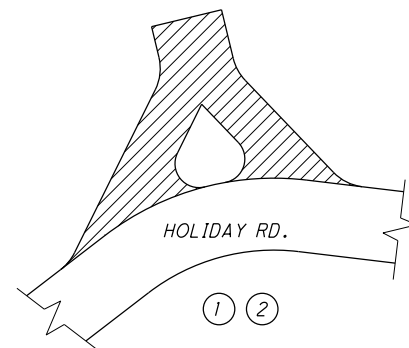
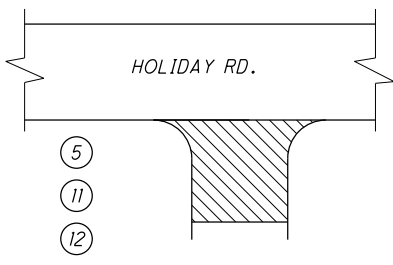
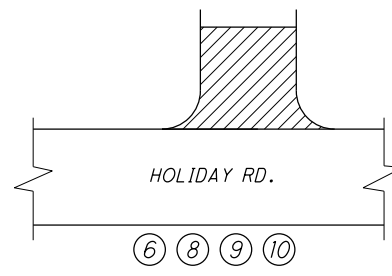
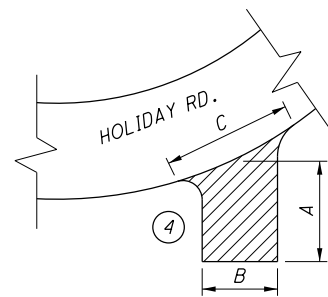
SHOULDER DETAIL



| LOCATION | | | | | PAVEMENT WIDTH | | | | | | | QUANTITIES | | | | | | REMARKS |
|-------------------------------------|-------|-------|-------|---------|----------------|------------------|----------|-----------------|-----------------|----------|------------------|------------------------------------|---------------|---------------------------------------|---|-------------------|---------------------------------|------------------------------|
| COUNTY | ROUTE | S L M | S L M | TYPICAL | NORTHBOUND | | | | SOUTHBOUND | | | 254 | 407 | 408 | 441 | 617 | 618 | REMARKS |
| | | | | | LENGTH | OUTSIDE SHOULDER | PAVEMENT | MEDIAN SHOULDER | MEDIAN SHOULDER | PAVEMENT | OUTSIDE SHOULDER | PAVEMENT PLANING, ASPHALT CONCRETE | TACK COAT | PRIME COAT (0.4 GAL/YD ²) | ASPHALT SURFACE COURSE, TYPE 1, (446), PG 64-22 | COMPACT AGGREGATE | RUMBLE STRIP (ASPHALT CONCRETE) | |
| | | | | | FT | FT | FT | FT | FT | FT | 1/2" DEPTH SY | GAL | GAL | 1/2" DEPTH CY | 2" DEPTH CY | FT | | |
| MRW | 71 | 3.17 | 12.07 | 1 | 46,992' | 10' | 24' | 5' | | | | 203,632 | 15,272 | 8,354 | 8,485 | 1,161 | 93,984 | MAINLINE (SOUTH OF SR-95) |
| MRW | 71 | 3.17 | 12.36 | 1 | 48,523' | | | | 5' | 24' | 10' | 210,267 | 15,770 | 8,626 | 8,761 | 1,198 | 97,046 | MAINLINE (SOUTH OF SR-95) |
| MRW | 71 | 13.44 | 19.38 | 1 | 31,363' | 10' | 24' | 5' | | | | 135,907 | 10,193 | 5,576 | 5,663 | 775 | 62,726 | MAINLINE (NORTH OF SR-95) |
| MRW | 71 | 13.57 | 19.38 | 1 | 30,677' | | | | 5' | 24' | 10' | 132,933 | 9,970 | 5,454 | 5,539 | 758 | 61,354 | MAINLINE (NORTH OF SR-95) |
| MRW | 71 | 10.99 | 11.14 | 1 | 792' | 10' | 32'* | | | | | 3,696 | 277 | | 154 | | | EXTRA AREA (RAMP TAPER) |
| MRW | 71 | 11.39 | 11.70 | 1 | 1,637' | 10' | 32'* | | | | | 7,638 | 573 | | 318 | | | EXTRA AREA (RAMP TAPER) |
| MRW | 71 | 11.33 | 11.48 | 1 | 792' | | | | | 32'* | 10' | 3,696 | 277 | | 154 | | | EXTRA AREA (RAMP TAPER) |
| MRW | 71 | 10.75 | 11.06 | 1 | 1,637' | | | | | 32'* | 10' | 7,638 | 573 | | 318 | | | EXTRA AREA (RAMP TAPER) |
| MRW | 71 | 4.41 | | | | | | | | | | 340 | 25 | | 14 | | | * AVERAGE PAVEMENT WIDTH |
| MRW | 71 | 6.67 | | | | | | | | | | | | | | 10 | | EXTRA AREA (PAVED CROSSOVER) |
| MRW | 71 | 7.40 | | | | | | | | | | 340 | 25 | | 14 | | | GRAVEL CROSSOVER |
| MRW | 71 | 10.37 | | | | | | | | | | 340 | 25 | | 14 | | | EXTRA AREA (PAVED CROSSOVER) |
| MRW | 71 | 11.98 | | | | | | | | | | 340 | 25 | | 14 | | | EXTRA AREA (PAVED CROSSOVER) |
| MRW | 71 | 13.63 | | | | | | | | | | 340 | 25 | | 14 | | | EXTRA AREA (PAVED CROSSOVER) |
| MRW | 71 | 14.67 | | | | | | | | | | | | | | 10 | | GRAVEL CROSSOVER |
| MRW | 71 | 15.06 | | | | | | | | | | | | | | 10 | | GRAVEL CROSSOVER |
| MRW | 71 | 15.60 | | | | | | | | | | | | | | 10 | | GRAVEL CROSSOVER |
| MRW | 71 | 16.75 | | | | | | | | | | 340 | 25 | | 14 | | | EXTRA AREA (PAVED CROSSOVER) |
| MRW | 71 | 17.52 | | | | | | | | | | | | | | 10 | | GRAVEL CROSSOVER |
| MRW | 71 | 18.03 | | | | | | | | | | | | | | 10 | | GRAVEL CROSSOVER |
| MRW | 71 | 5.96 | 6.01 | | 255' | 10' | 24' | 5' | 5' | 24' | 10' | -2,210 | -2 | -45 | -92 | -6 | -510 | DEDUCTIONS & EXTRA AREAS |
| MRW | 71 | 7.05 | 7.07 | | 120' | 10' | 24' | 5' | 5' | 24' | 10' | -1,039 | 4 | -21 | 2 | -3 | -240 | DEDUCTIONS & EXTRA AREAS |
| MRW | 71 | 7.56 | 7.59 | | 145' | 10' | 24' | 5' | 5' | 24' | 10' | -1,254 | 5 | -26 | 3 | -4 | -289 | DEDUCTIONS & EXTRA AREAS |
| MRW | 71 | 12.17 | 12.19 | | 103' | 10' | 24' | 5' | 5' | 24' | 10' | -892 | 3 | -18 | 2 | -3 | -206 | DEDUCTIONS & EXTRA AREAS |
| MRW | 71 | 15.50 | 15.52 | | 80' | 10' | 24' | 5' | 5' | 24' | 10' | -691 | 3 | -14 | 1 | -2 | -159 | DEDUCTIONS & EXTRA AREAS |
| MRW | 71 | 17.63 | 17.68 | | 257' | 10' | 24' | 5' | 5' | 24' | 10' | -2,224 | 9 | -46 | 5 | -6 | -513 | DEDUCTIONS & EXTRA AREAS |
| MRW | 71 | 18.77 | 18.80 | | 155' | 10' | 24' | 5' | 5' | 24' | 10' | -1,341 | 5 | -28 | 3 | -4 | -309 | DEDUCTIONS & EXTRA AREAS |
| TOTALS CARRIED TO SUBSUMMARY | | | | | | | | | | | | 697,797 | 53,082 | 27,812 | 29,400 | 3,924 | 312,883 | |

PAVEMENT SUBSUMMARY AND DETAILS

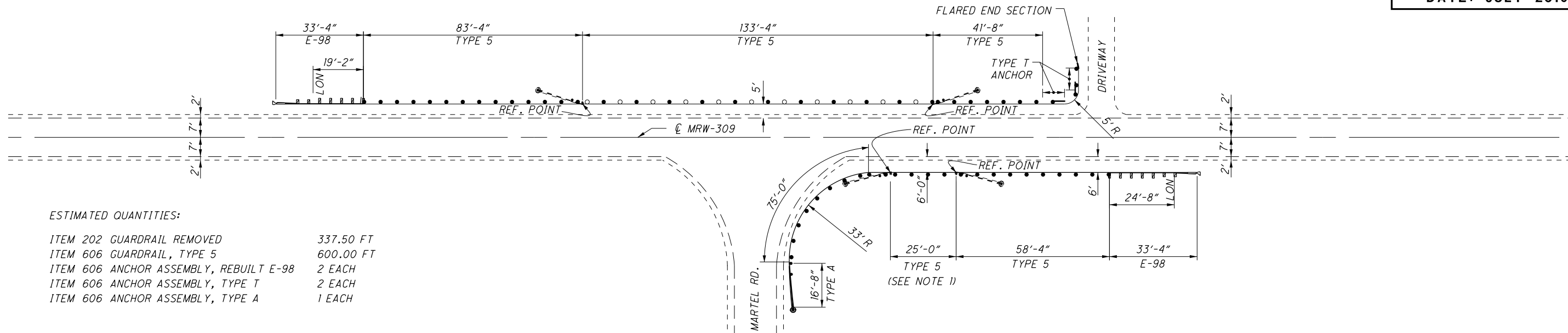
MRW-71-3.17



| PART | ROUTE | SLM | SIDE | DESCRIPTION | INTERSECTIONS | | | | PROPOSED ITEMS | | | | | | |
|------------------------------|--------|-------------|------|----------------------------|---------------|----|-----|--------|----------------|------------------|-----------------|-----------------|------------------|-------------------|--|
| | | | | | A | B | C | AREA | 407 | ASPHALT CONCRETE | | | EXISTING SURFACE | 408 | |
| | | | | | | | | | TACK COAT | THICKNESS | ITEM 441 TYPE 1 | ITEM 441 TYPE 2 | | THICKNESS | BITUMINOUS PRIME COAT (@ 0.40 GAL/YD²) |
| FT | FT | FT | YD² | GAL | INCHES | CY | CY | INCHES | GAL | | | | | | |
| | CR-444 | 0.00 - 5.88 | LT | ① POOR RD. (T-304) | 182 | 17 | 59 | 1,194 | | 1.00 | 33 | 58 | 1.75 | GRAVEL/TAR & CHIP | 478 |
| | | | LT | ② POOR RD. SPUR (T-304) | 43 | 17 | 43 | 206 | | 1.00 | 6 | 10 | 1.75 | GRAVEL/TAR & CHIP | 82 |
| | | | RT | ③ VISTA RD. (T-223) | 39 | 20 | 85 | 369 | 28 | 1.00 | 10 | 18 | 1.75 | ASPHALT | |
| | | | RT | ④ TURNER RD. (T-156) | 35 | 17 | 65 | 253 | | 1.00 | 7 | 12 | 1.75 | GRAVEL | 101 |
| | | | RT | ⑤ BASIL RD. (T-155/CRI56) | 70 | 18 | 120 | 934 | 70 | 1.00 | 26 | 45 | 1.75 | ASPHALT | |
| | | | LT | ⑥ WOODY RD. (T-155) | 30 | 16 | 54 | 180 | 14 | 1.00 | 5 | 9 | 1.75 | ASPHALT | |
| | | | LT | ⑦ McCORD RD. SPUR (T-155A) | 70 | 12 | 145 | 1,128 | | 1.00 | 31 | 55 | 1.75 | GRAVEL | 451 |
| | | | LT | ⑧ BUMP RD. (T-215) | 30 | 18 | 70 | 234 | 17 | 1.00 | 7 | 11 | 1.75 | ASPHALT | |
| | | | LT | ⑨ ROUND CIRCLE | 33 | 18 | 50 | 184 | 14 | 1.00 | 5 | 9 | 1.75 | ASPHALT | |
| | | | LT | ⑩ GRAPE RD. (T-485) | 30 | 18 | 95 | 317 | 24 | 1.00 | 9 | 15 | 1.75 | ASPHALT | |
| | | | RT | ⑪ SOUTHERN DR. | 24 | 18 | 40 | 107 | 8 | 1.00 | 3 | 5 | 1.75 | ASPHALT | |
| | | | RT | ⑫ CHIP RD. (T-316) | 60 | 18 | 96 | 640 | 48 | 1.00 | 18 | 31 | 1.75 | ASPHALT | |
| | | | LT | ⑬ HOLIDAY RD./SR 32 | 60 | 26 | 112 | 747 | 56 | 1.00 | 21 | 36 | 1.75 | ASPHALT | |
| TOTAL | | | | | | | | 6,493 | | | | | | | |
| TOTALS (CARRIED TO SHEET 10) | | | | | | | | | 279 | | 181 | 314 | | | 1,112 |

CALCULATIONS

KNO - 444 - 0.00



ESTIMATED QUANTITIES:

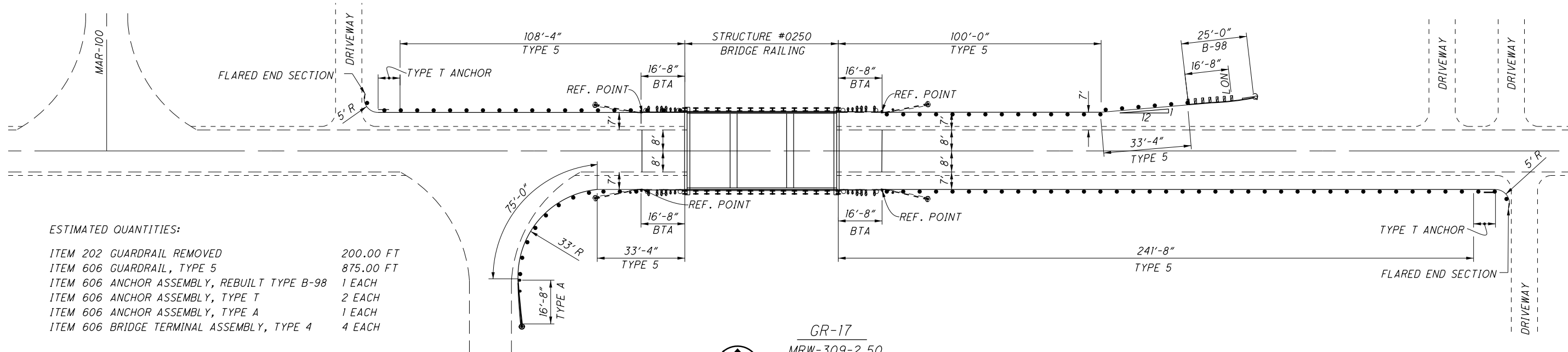
| | |
|--|-----------|
| ITEM 202 GUARDRAIL REMOVED | 337.50 FT |
| ITEM 606 GUARDRAIL, TYPE 5 | 600.00 FT |
| ITEM 606 ANCHOR ASSEMBLY, REBUILT E-98 | 2 EACH |
| ITEM 606 ANCHOR ASSEMBLY, TYPE T | 2 EACH |
| ITEM 606 ANCHOR ASSEMBLY, TYPE A | 1 EACH |

ALL QUANTITIES CARRIED TO SUBSUMMARY SHEET 8.

ALL QUANTITIES CARRIED TO GENERAL SUMMARY SHEET 7.

GR-16
MRW-309-2.37

NOTE:
REMOVE AS PER ITEM 202, GUARDRAIL REMOVED.
REPLACE WITH ITEM 606, GUARDRAIL, TYPE 5.



ESTIMATED QUANTITIES:

| | |
|---|-----------|
| ITEM 202 GUARDRAIL REMOVED | 200.00 FT |
| ITEM 606 GUARDRAIL, TYPE 5 | 875.00 FT |
| ITEM 606 ANCHOR ASSEMBLY, REBUILT TYPE B-98 | 1 EACH |
| ITEM 606 ANCHOR ASSEMBLY, TYPE T | 2 EACH |
| ITEM 606 ANCHOR ASSEMBLY, TYPE A | 1 EACH |
| ITEM 606 BRIDGE TERMINAL ASSEMBLY, TYPE 4 | 4 EACH |

ALL QUANTITIES CARRIED TO SUBSUMMARY SHEET 8.

ALL QUANTITIES CARRIED TO GENERAL SUMMARY SHEET 7.

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